



City of Port Townsend Holcomb & Wilson Sewer Replacement

90% SUBMITTAL

SECTION AND DETAIL REFERENCES

THE FOLLOWING CONVENTIONS HAVE BEEN USED WITHIN THESE DRAWINGS TO REFER THE READER BETWEEN THE SECTION/DETAIL AND THE PLAN FROM WHICH IT IS REFERENCED.

REFERENCE BUBBLES



PLAN REFERENCE BUBBLE - REFERS READER BACK TO THE PLAN FROM WHICH THE DETAIL OR SECTION ORIGINATED.



DETAIL/SECTION REFERENCE BUBBLE - REFERS READER TO THE DRAWING ON WHICH THE DETAIL OR SECTION IS LOCATED.

WHERE, X = SECTION/DETAIL REFERENCE ID*
X## = DRAWING NUMBER ON WHICH DETAIL ORIGINATED OR RESIDES.

*SECTION/DETAIL REFERENCE ID CONVENTIONS:
SECTIONS OR ELEVATIONS SHOULD HAVE A LETTER REFERENCE ID (A - ZZ) AND DETAILS SHOULD HAVE A NUMERICAL REFERENCE ID (0 - 999)

CONTACT PERSONNEL

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SURVEY NOTES

BASIS OF BEARINGS: WASHINGTON COORDINATE SYSTEM OF 1982, ADJUSTMENT 1991, NORTH ZONE, NORTH AMERICAN DATUM (NAD 83/91), DETERMINED FROM FIELD MEASUREMENTS TO CITY OF PORT TOWNSEND GEODETIC CONTROL POINTS #0011071 AND #0011072.

VERTICAL DATUM: NORTH AMERICAN VERTICAL DATUM (NAVD) OF 1988 ESTABLISHED BY FIELD TIES TO CITY OF PORT TOWNSEND GEODETIC CONTROL POINTS #0011071 AND #0011072.

THE MAP IS BASED ON FIELD SURVEY CONDUCTED DURING NOVEMBER 2023 AND JULY 2025.

UNDERGROUND UTILITIES SHOWN HEREON ARE BASED UPON FIELD LOCATED STRUCTURES AND THE CITY OF PORT TOWNSEND GIS MAP AND MUST BE VERIFIED PRIOR TO CONSTRUCTION. NO UNDERGROUND UTILITY LOCATED WERE PERFORMED PER THIS SURVEY.

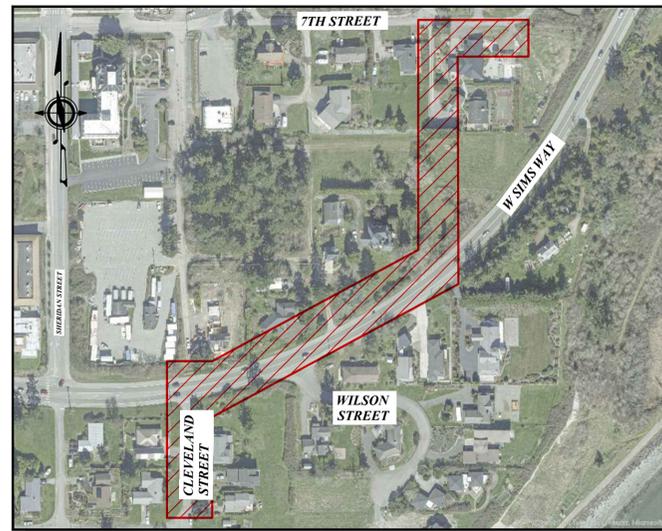
**CALL 48 HOURS BEFORE YOU DIG
ONE CALL 811**

**REPORT ALL SPILLS
DEPT. OF ECOLOGY 1-800-258-5990**

PROJECT VICINITY MAP



PROJECT LOCATION MAP



DRAWING INDEX

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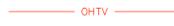
ABBREVIATIONS

AC	ASBESTOS CEMENT	LTF	LENGTH TO FIT
CB	CATCH BASIN	MH	MANHOLE
CONC	CONCRETE	MIN	MINIMUM
CL	CENTERLINE	MUTCD	MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES
CSBC	CRUSHED SURFACING BASE COURSE	N	NORTHING
CSTC	CRUSHED SURFACING TOP COURSE	PE	POLYETHYLENE
DIAM	DIAMETER	PROP	PROPOSED
DI	DUCTILE IRON	PVC	POLYVINYL CHLORIDE
DWG	DRAWING	ROW	RIGHT-OF-WAY
E	EASTING	SPEC	SPECIFICATIONS
ELEV	ELEVATION	SS	SANITARY SEWER
EOP	EDGE OF PAVEMENT	SSMH	SANITARY SEWER MANHOLE
EX	EXISTING	ST	STORM
FM	FORCEMAIN	STA	STATION LINE
HDPE	HIGH DENSITY POLYETHYLENE	STD	STANDARD
HMA	HOT MIXED ASPHALT	SY	SQUARE YARDS
ID	INNER DIAMETER	TYP	TYPICAL
LF	LINEAR FEET	W	WATER

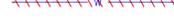


LEGEND

EXISTING LEGEND

	UNDERGROUND CABLE
	OVERHEAD CABLE
	UNDERGROUND TELEPHONE
	UNDERGROUND FIBER OPTICS
	OVERHEAD TELEPHONE
	NATURAL GAS
	PETROLEUM
	UNDERGROUND POWER
	OVERHEAD POWER
	SANITARY SEWER
	SEWER FORCEMAIN
	STORM DRAIN CULVERT
	STORM DRAIN DITCH
	STORM DRAIN LINE
	WATER LINE
	WATER SERVICE LINE

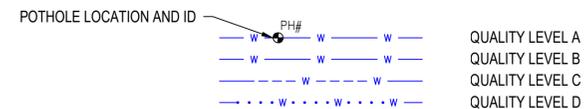
PROPOSED LEGEND

	GRAVITY SEWER PIPE
	SANITARY SEWER MANHOLE
	WATER LINE TO BE ABANDONED
	WATER LINE TO BE REMOVED
	TREE PROTECTION FENCING
	WATER LINE
	APPROX. TRENCH LIMITS
	FIRE HYDRANT (3 PORT)
	WATER GATE VALVE
	WATER AIR RELIEF VALVE

GENERAL NOTES

- ALL WORKMANSHIP, CONSTRUCTION AND MATERIALS SHALL BE PERFORMED OR SUPPLIED IN ACCORDANCE WITH THESE SPECIAL PROVISIONS, PLANS, CITY STANDARD DETAILS, AND THE WSDOT STANDARD SPECIFICATIONS FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION, LATEST EDITION, AS ISSUED BY THE WASHINGTON STATE DEPARTMENT OF TRANSPORTATION AND THE AMERICAN PUBLIC WORKS ASSOCIATION, WHICH IS HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS.
- A PRECONSTRUCTION CONFERENCE IS REQUIRED PRIOR TO CONSTRUCTION, AND 48 HOURS ADVANCE NOTIFICATION PRIOR TO ACTUAL START OF WORK IS REQUIRED.
- THE EXISTING TOPOGRAPHIC AND PHYSICAL FEATURES SHOWN ON THESE PLANS ARE BASED ON A FIELD SURVEY BY VAN ALLER SURVEYING, RECORD DRAWINGS, GIS INFORMATION, AND FIELD RECONNAISSANCE BY RH2 ENGINEERING.
- THE LOCATIONS AND ELEVATIONS OF EXISTING UNDERGROUND UTILITIES SHOWN ON THE PLANS WERE OBTAINED FROM AVAILABLE RECORDS BUT HAVE NOT BEEN EXPOSED AND MEASURED. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES PRIOR TO COMMENCING WORK TO AVOID DAMAGE OR DISTURBANCE, AND AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE CAUSED BY THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ALL UNDERGROUND UTILITIES. IT IS UNDERSTOOD THAT OTHER ABOVE GROUND AND UNDERGROUND FACILITIES NOT SHOWN ON THE PLANS MAY BE ENCOUNTERED DURING THE COURSE OF THE WORK.
- THE CONTRACTOR SHALL PROTECT BUILDINGS, FENCES, APPURTENANCES, ABOVE GROUND UTILITIES, AND OTHER PROPERTY ADJACENT TO ALL CONSTRUCTION AREAS. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR REPAIRING ALL DAMAGE CAUSED BY CONSTRUCTION ACTIVITIES.
- IN ACCORDANCE WITH THE DEPARTMENT OF ECOLOGY AIR QUALITY STANDARDS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTROLLING ALL FUGITIVE DUST THAT MAY BE GENERATED BY THE CONSTRUCTION PROJECT.
- THE CONTRACTOR SHALL CONTAIN WORK TO WITHIN RIGHT-OF-WAY OR THE CONSTRUCTION LIMITS AS ILLUSTRATED IN THE PLANS.
- THE CONTRACTOR SHALL SECURE NECESSARY PERMITS PRIOR TO STARTING CONSTRUCTION. THE CITY WILL OBTAIN SOME OF THE REQUIRED PERMITS. SEE TECHNICAL SPECIFICATIONS FOR FURTHER INFORMATION REGARDING PERMITS.
- ONSITE EROSION CONTROL MEASURES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND BE IN PLACE PRIOR TO CONSTRUCTION. ANY PROBLEMS OCCURRING BEFORE FINAL ACCEPTANCE BY THE CITY SHALL BE CORRECTED BY THE CONTRACTOR. UPON FINAL ACCEPTANCE BY THE CITY, OR AS OTHERWISE DIRECTED BY THE ENGINEER, THE CONTRACTOR SHALL REMOVE ALL TEMPORARY, NON-DEGRADABLE EROSION CONTROL MEASURES.
- ANY REVISIONS TO PLANS MUST BE MADE BY THE ENGINEER AND APPROVED BY THE CITY PRIOR TO ANY IMPLEMENTATION IN THE FIELD.
- ALL PAVEMENT MARKINGS AND SIGNING SHALL CONFORM TO THE REQUIREMENTS OF THE MUTCD.
- A COPY OF THE APPROVED PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
- WHERE NEWLY CONSTRUCTED PAVING MEETS EXISTING PAVING, THE CONTRACTOR SHALL SAW CUT OR OVERLAY AND GRADE NEW PAVEMENT TO PROVIDE A SMOOTH TRANSITION FROM EXISTING TO PROPOSED PAVING. APPLICATION OF A THIN TACK COAT OF EMULSIFIED ASPHALT SHALL BE APPLIED TO INSURE PROPER BONDING.
- THE COMPLETED SURFACE OF ALL COURSES SHALL BE OF UNIFORM TEXTURE, SMOOTH, UNIFORM AS TO CROWN AND GRADE, AND FREE FROM DEFECTS OF ALL KINDS. THE COMPLETED SURFACE OF THE WEARING COURSE SHALL NOT VARY MORE THAN 1/8 INCH FROM THE LOWER EDGE OF A 10-FOOT STRAIGHTEDGE PLACED ON THE SURFACE PARALLEL TO THE CENTERLINE. THE TRANSVERSE SLOPE OF THE COMPLETED SURFACE OF THE WEARING COURSE SHALL VARY NOT MORE THAN 1/4 INCH IN 10- FEET FROM THE RATE OF TRANSVERSE SLOPE SHOWN ON THE PLANS.
- MATERIALS SAMPLING AND TESTING SHALL BE AT A FREQUENCY AND MAGNITUDE AS SPECIFIED IN THE STANDARD SPECIFICATIONS OR DETERMINED BY THE ENGINEER. A PRIVATE AND INDEPENDENT TESTING LABORATORY SHALL PERFORM TESTING AND SAMPLING. CERTIFIED TEST REPORTS SHALL BE FURNISHED FOR ALL TESTS PERFORMED BY PRIVATE TESTING LABORATORIES. THE CITY WILL BE RESPONSIBLE FOR ACCEPTANCE TESTING.

SUBSURFACE UTILITY LEGEND



THE CLASSIFICATIONS FOR SUBSURFACE UTILITIES ARE OUTLINED AND EXPLAINED IN THE FOLLOWING LIST:

UTILITY QUALITY LEVEL A - PRECISE HORIZONTAL AND VERTICAL LOCATION OF UTILITIES OBTAINED BY THE ACTUAL EXPOSURE OF (OR VERIFICATION OF PREVIOUSLY EXPOSED AND SURVEYED UTILITIES) AND SUBSEQUENT MEASUREMENT OF SUBSURFACE UTILITIES, USUALLY AT A SPECIFIC POINT. UNLESS OTHERWISE NOTED, QUALITY LEVEL A IS ONLY APPLICABLE AT POTHOLED LOCATIONS ON THE PLANS AT ALL OTHER AREAS, THE UTILITY SHOULD BE ASSUMED TO BE QUALITY LEVEL B.

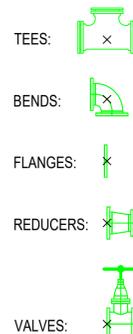
UTILITY QUALITY LEVEL B - INFORMATION OBTAINED THROUGH THE APPLICATION OF APPROPRIATE SURFACE GEOPHYSICAL METHODS TO DETERMINE THE EXISTENCE AND APPROXIMATE HORIZONTAL POSITION OF SUBSURFACE UTILITIES.

UTILITY QUALITY LEVEL C - INFORMATION OBTAINED BY SURVEYING AND PLOTTING VISIBLE ABOVE-GROUND UTILITY FEATURES

UTILITY QUALITY LEVEL D - INFORMATION DERIVED FROM EXISTING RECORDS OR ORAL RECOLLECTIONS.

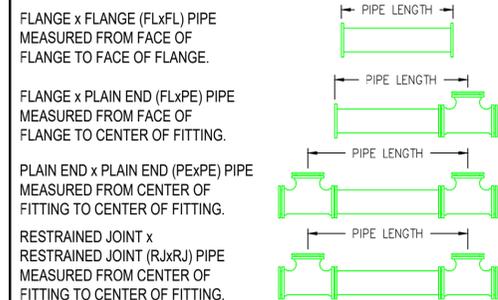
NOTE: THE USE OF THE LINETYPES PROVIDED ABOVE WAS A PRIMARY METHOD FOR INDICATING THE ACCURACY OF THE UTILITIES SHOWN WITHIN THESE PLANS. WHEN THE SOURCE OF THE INFORMATION WAS UNKNOWN OR THE METHOD FOR LOCATING THE UTILITIES WAS UNAVAILABLE, QUALITY LEVEL D WAS USED AS THE DEFAULT.

NORTHING AND EASTING CALLOUT POINTS



PIPE LENGTH MEASUREMENTS

PIPE LENGTHS CALLED OUT ON PLANS ARE MEASURED AS FOLLOWS:



FITTINGS ARE ASSUMED TO BE STANDARD LENGTH 125#, 250# FLANGED OR COMPACT CLASS 350 MECHANICAL JOINTS. CONTRACTOR RESPONSIBLE FOR VERIFYING LENGTHS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO TAKE INTO ACCOUNT ANY VARIATIONS IN FITTING DIMENSIONS.



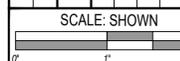
CITY OF PORT TOWNSEND
HOLCOMB AND WILSON
SEWER REPLACEMENT

GENERAL NOTES



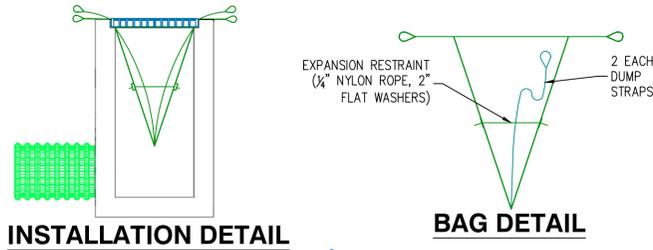
ENGINEER: CRB	DATE: Jan 22, 2026	CLIENT: TOWNSEND	JOB NO.: 25-0104	NO.	DATE	DESCRIPTION	BY	REVIEW
REVIEWED: RJF	DATE: Jan 23, 2026	FILENAME: HOL-D-COV.DWG						
REVISIONS								

SCALE: SHOWN



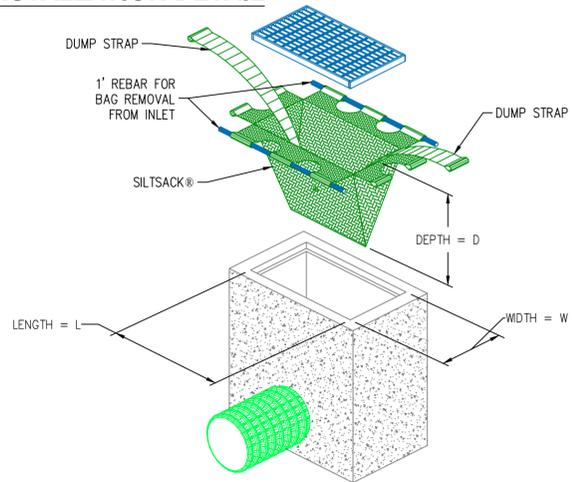
DRAWING IS FULL SCALE WHEN BAR MEASURES 2'

DWG NO.: G01	SHEET NO.: 2	19
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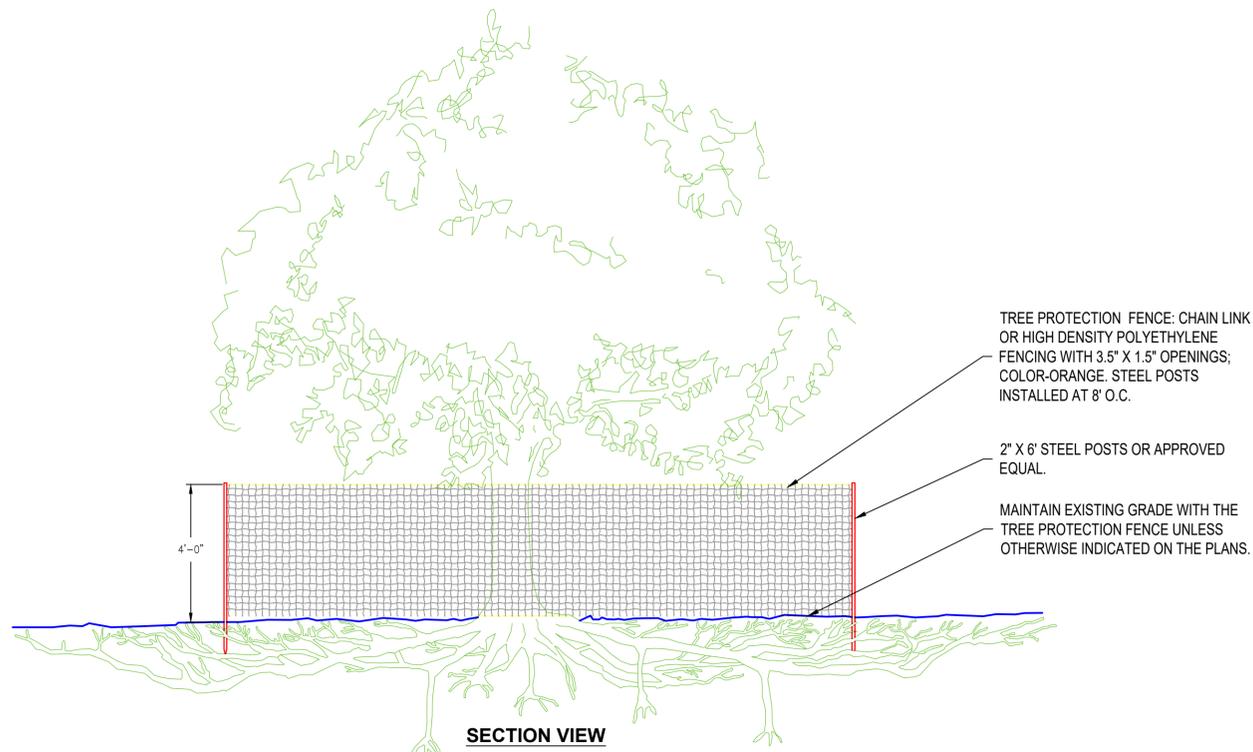
INSTALLATION DETAIL

BAG DETAIL



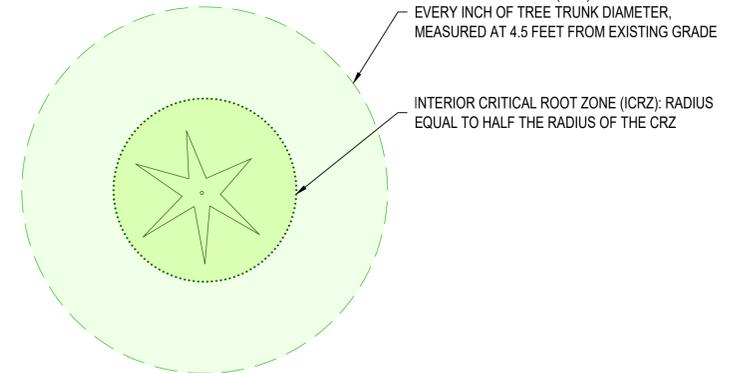
CATCH BASIN SILTSACK DETAIL
NOT TO SCALE

101
TYP.



TREE PROTECTION FENCING
NTS

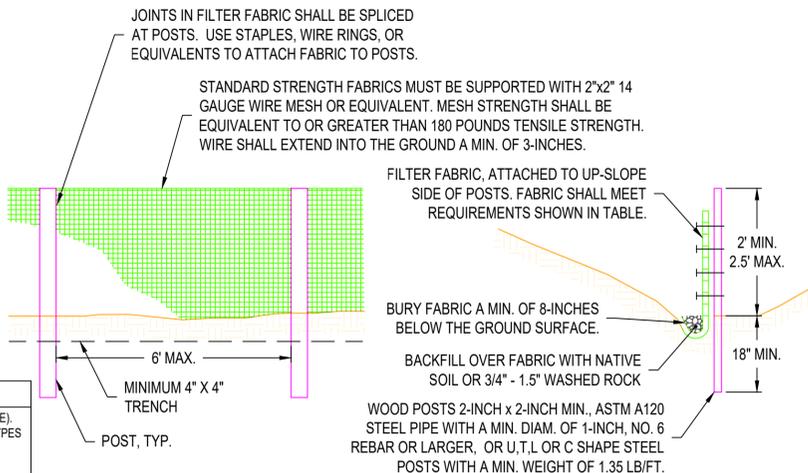
110
TYP.



TREE CRITICAL ROOT ZONE
NOT TO SCALE

112
TYP.

- SILT FENCE NOTES:**
- FILTER FABRIC MATERIAL SHALL CONTAIN ULTRAVIOLET RAY INHIBITORS AND STABILIZERS TO PROVIDE A MINIMUM OF 6 MONTHS OF EXPECTED USABLE CONSTRUCTION LIFE AT A TEMPERATURE RANGE OF 0°F TO 120°F.
 - FILTER FABRIC FENCES SHALL BE INSTALLED AS SHOWN PER PLAN. ADJUST AS NECESSARY TO FOLLOW CONTOUR WHENEVER POSSIBLE. THE ENDS OF THE FENCE SHALL BE TURNED UP INTO THE CONTOUR.
 - FILTER FABRIC SHALL BE SEWN TOGETHER AT THE POINT OF MANUFACTURE TO FORM FILTER FABRIC LENGTHS AS REQUIRED. ALTERNATIVELY TWO SECTIONS OF FENCE CAN BE OVERLAPPED, PROVIDE THAT THE CONTRACTOR CAN DEMONSTRATE, TO THE SATISFACTION OF THE ENGINEER, THAT THE OVERLAP IS LONG ENOUGH AND THAT THE ADJACENT FENCE SECTIONS ARE CLOSE ENOUGH TOGETHER TO PREVENT SILT LADEN WATER FROM ESCAPING THE FENCE OVERLAP.



SILT FENCE
NOT TO SCALE

111
TYP.

FILTER FABRIC	
POLYMERIC MESH AOS (ASTM D4751)	0.60 MM MAXIMUM FOR FILM WOVENS (US #30 SIEVE), 0.30 MM MAXIMUM FOR ALL OTHER GEOTEXTILE TYPES (US #50 SIEVE), 0.15 MM MINIMUM FOR ALL FABRIC TYPES (US #100 SIEVE).
WATER PERMITTIVITY (ASTM D4491)	0.02 SEC ⁻¹ MINIMUM
GRAB TENSILE STRENGTH (ASTM D4632)	180 LBS. MINIMUM FOR EXTRA STRENGTH FABRIC, 100 LBS. MINIMUM FOR STANDARD STRENGTH FABRIC.
GRAB TENSILE ELONGATION (ASTM D4632)	30% MAXIMUM.
ULTRAVIOLET RESISTANCE (ASTM D4355)	70% MINIMUM.

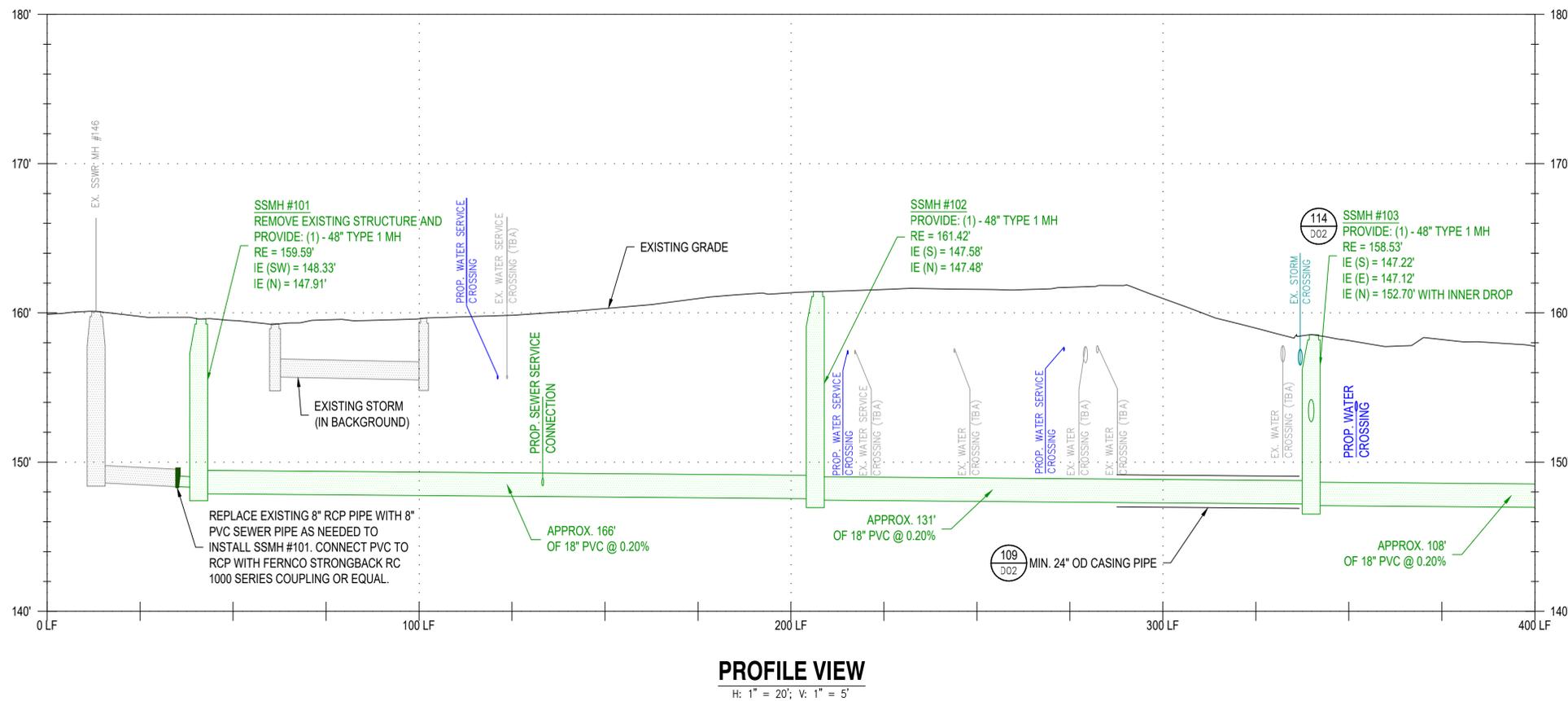
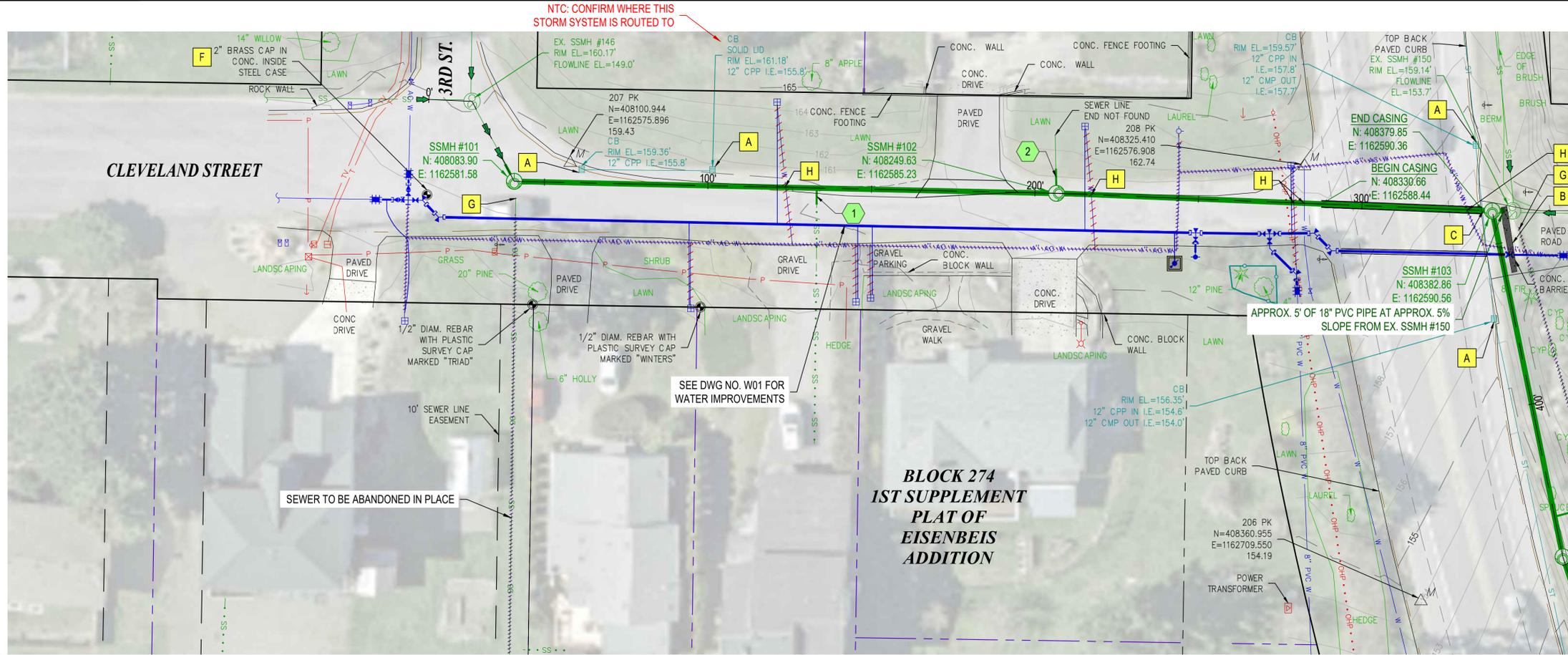
- TREE PROTECTION NOTES**
- NO EXCAVATION OR EQUIPMENT IS ALLOWED IN THE INTERIOR CRITICAL ROOT ZONE (ICRZ) WITHOUT PUBLIC WORKS APPROVAL.
 - TREE PROTECTION FENCING SHALL BE IN PLACE BEFORE EXCAVATION BEGINS.
 - PRIOR TO PERFORMING WORK WITHIN THE CRITICAL ROOT ZONE (CRZ), AN ON-SITE MEETING WITH THE CITY/ENGINEER IS REQUIRED TO IDENTIFY ALLOWABLE WORK WITHIN THE CRZ, ALLOWABLE TREES TO BE REMOVED, AND TO CONFIRM TREE PROTECTION MEASURES ARE IN COMPLIANCE WITH THE CONTRACT DOCUMENTS.
 - WHERE SHOWN ON PLANS AND WHERE WORK IS TO BE PERFORMED WITHIN THE CRZ, PROVIDE TREE PROTECTION FENCING AS SHOWN ON THIS SHEET. FENCE SHALL BE INSTALLED AT ICRZ BOUNDARY.
 - EXCAVATION WITH PRESSURIZED AIR OR WATER IS REQUIRED AROUND ROOTS. WHERE FEASIBLE, TUNNEL UNDER ROOTS.
 - ROOTS LARGER THAN 1" SHALL BE CUT WITH HAND TOOLS OR SAWS.
 - REMOVAL OF ROOTS LARGER THAN 4" REQUIRES ARBORIST, CITY, OR ENGINEER APPROVAL. FOR REMOVAL OF TREES, REFER TO SPECIFICATIONS.
 - WHERE ROOTS ARE IMPACTED DURING CONSTRUCTION, POST CARE TREATMENT SUCH AS REGULAR IRRIGATION, MISTING, ROOT TREATMENT WITH ROOT HORMONES, AND/OR MULCHING IS REQUIRED. SUBMIT A POST CARE TREATMENT PLAN FOR CITY APPROVAL PRIOR TO PERFORMING ANY WORK WITHIN THE CRZ.

CITY OF PORT TOWNSEND
HOLCOMB AND WILSON
SEWER REPLACEMENT



TESC AND TREE PROTECTION DETAILS

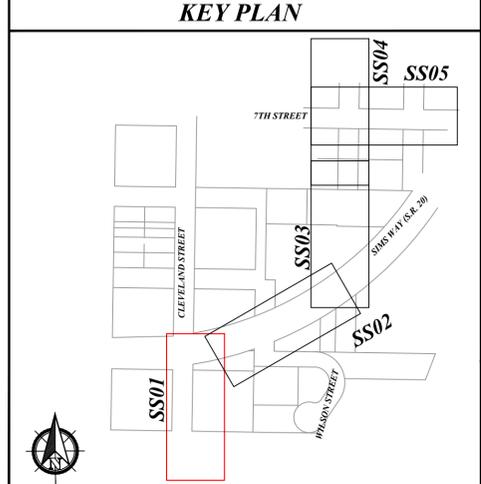
NO.	DATE	DESCRIPTION	BY	REVIEW



- GENERAL NOTES**
- ALL SIDE SEWERS SHALL BE CONSTRUCTED PER CITY STANDARD DETAILS. A MINIMUM SLOPE OF 2% IS REQUIRED. IF MINIMUM SLOPE CAN NOT BE ACHIEVED, THE CONTRACTOR SHALL COORDINATE WITH THE CITY ON A MINIMUM ACHIEVABLE SLOPE.
 - EXISTING UTILITIES HAVE NOT BEEN POTHOLED TO IDENTIFY LOCATION, MATERIAL, SIZE, OR GEOMETRY. POT HOLE EXISTING UTILITY CROSSINGS TO VERIFY LOCATION, MATERIAL, SIZE, AND DEPTH PRIOR TO INSTALLING PROPOSED MAIN.
 - CONFIRM ELEVATIONS AT ALL EXISTING MANHOLES WITH PROPOSED CONNECTIONS PRIOR TO ORDERING PARTS. RIM ELEVATIONS AND INVERTS HAVE BEEN APPROXIMATED.
 - SEE DWG. D03 FOR ROADWAY RESTORATION DETAILS.
 - SEWER FLOW INTERRUPTIONS SHALL BE MANAGED BY TEMPORARY BYPASSES OR OTHER APPROVED ALTERNATIVE.
 - ALL SIDE SEWER CONNECTIONS HAVE BEEN APPROXIMATED TO INTERCEPT THE SIDE SEWER FROM EACH HOME. THE CONTRACTOR SHALL VERIFY THE LOCATION AND DEPTH OF THE SIDE SEWER BY POTHOLES. THE CONTRACTOR SHALL COORDINATE WITH THE CITY IF THE APPROXIMATE CONNECTION POINT IS NOT ACHIEVABLE.
 - ALL DISTURBED AREA SHALL BE RESTORED TO EXISTING OR BETTER CONDITIONS PER RESTORATION SHEETS.
 - PROVIDE ETHAFOAM PAD EQUAL TO DOW ETHAFOAM 220 BETWEEN CROSSING UTILITIES WITH FEWER THAN 6" OF CLEARANCE. PAD SHALL FILL THE ENTIRE GAP.
 - PROVIDE SITE SECURITY FOR OPEN EXCAVATIONS INCLUDING, BUT NOT LIMITED TO, CONSTRUCTION FENCING, STEEL PLATES, AND BARRICADES FOR OPEN EXCAVATIONS AT ALL TIMES WHEN CONTRACTOR IS NOT ON-SITE.
 - ONCE BYPASS IS FUNCTIONAL AND APPROVED BY CITY, REMOVE EXISTING SEWER PIPE AS ENCOUNTERED WITHIN TRENCH. GROUT ABANDONED PIPE ENDS WITH PRECISION NON-SHRINK GROUT. GROUT PLUG LENGTH SHALL BE 2 TIMES PIPE DIAMETER.

- SIDE SEWER NOTES**
- 1 SIDE SEWER RECONNECTION (SS-1A D02)
 - 2 SIDE SEWER RECONNECTION (SS-1A D02)

- KEY NOTES**
- A** INSERT CATCH BASIN SILT SACK PER DETAIL ON DWG NO. D01. PROTECT CATCH BASIN AS NECESSARY DURING CONSTRUCTION OF PROPOSED IMPROVEMENTS. CLEAN OR REPLACE INLET PROTECTION DEVICES WHEN SEDIMENT HAS FILLED ONE-THIRD OF THE AVAILABLE STORAGE. IF CATCH BASIN IS DAMAGED DURING CONSTRUCTION, REMOVE AND REPLACE CATCH BASIN WITH AN APPROVED EQUAL.
 - B** REMOVE EXISTING CONCRETE BARRIER AND REPLACE WITH ONE PRECAST TYPE F CONCRETE BARRIER PER WSDOT STANDARD PLAN C-60.10-04.
 - C** REMOVE AND REPLACE STORM CATCH BASIN AND STORM PIPE AS NECESSARY TO COMPLETE SEWER IMPROVEMENTS. IF ELEMENTS OF THE STORM SYSTEM ARE OUT OF SERVICE DURING CONSTRUCTION, INSTALL TEMPORARY STORMWATER BYPASS SYSTEM TO CONVEY STORMWATER TO NEAREST CATCH BASIN OR OTHER DISCHARGE POINT APPROVED BY THE CITY. IF REQUIRED, REPLACEMENT CATCH BASIN SHALL BE A WSDOT TYPE 1 CATCH BASIN. REPLACEMENT STORM PIPE SHALL MATCH EXISTING PIPE MATERIAL OR BE CPP IN COMPLIANCE WITH WSDOT STANDARD SPECIFICATIONS SECTION 9-05.1(7).
 - F** PRESERVE MONUMENT DURING CONSTRUCTION. SEE DWG NO. R01 FOR ADDITIONAL INFORMATION.
 - G** REMOVE EXISTING PIPE WITHIN TRENCH LIMITS. GROUT PIPE END WITH NON-SHRINK GROUT EQUAL TO PRECISION NON-SHRINK GROUT. EXISTING PIPE SHALL BE PLUGGED FOR A LENGTH OF 2 PIPE DIAMETERS.
 - H** SEE DWG NO. D05 FOR WATER MAIN PHASING INSTALLATION AND SERVICE PIPE REPLACEMENT PHASING INFORMATION. WATER SERVICES SHALL BE MAINTAINED DURING CONSTRUCTION OF SEWER IMPROVEMENTS.



RH2

PRELIMINARY

CITY OF PORT TOWNSEND
HOLCOMB AND WILSON
SEWER REPLACEMENT

SEWER MAIN PLAN AND PROFILE 1

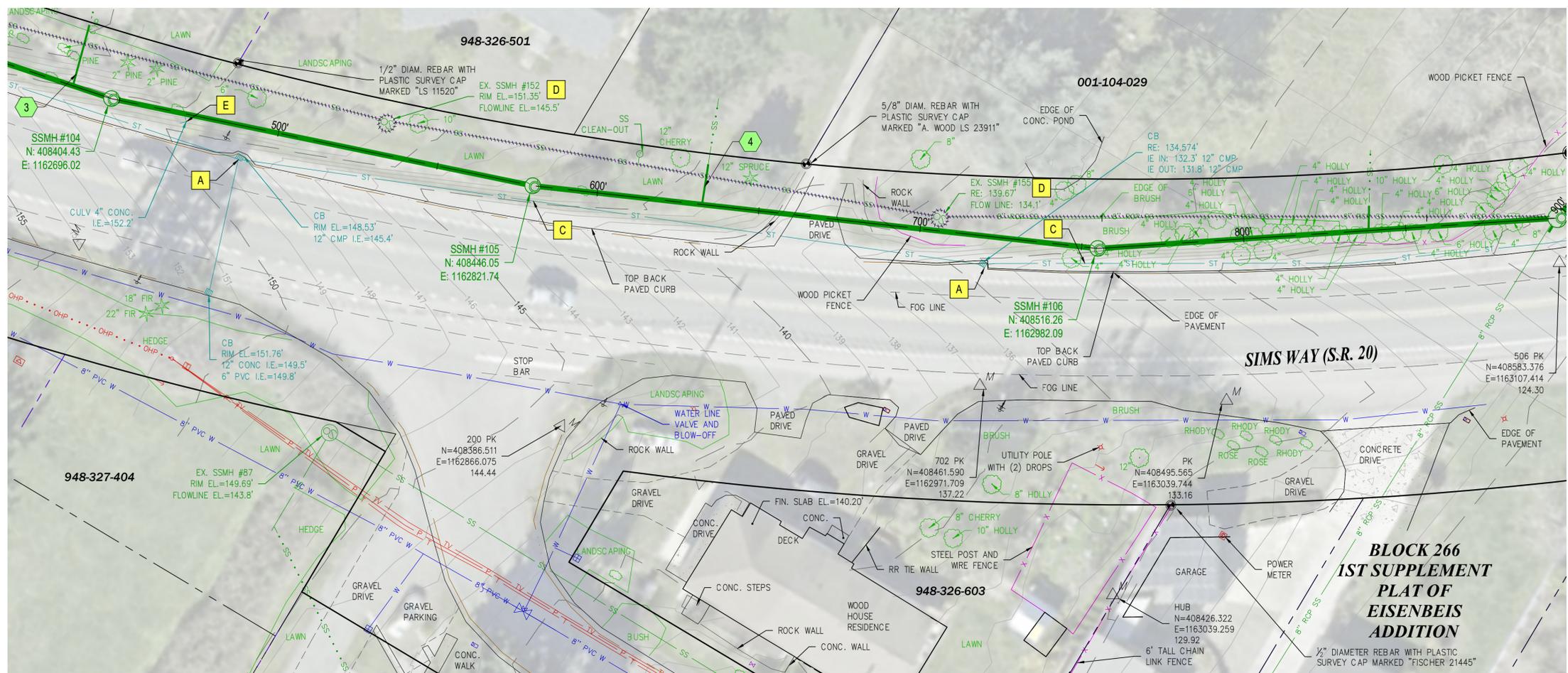
ENGINEER: CRB
DATE: Jan 23, 2026
PROJECT: TWNSD
CLIENT: TWNSD
JOB NO.: 25-0104
REVISIONS: HOL-D-SEWERP-NORTHDWG

NO.	DATE	DESCRIPTION	BY	REVIEW

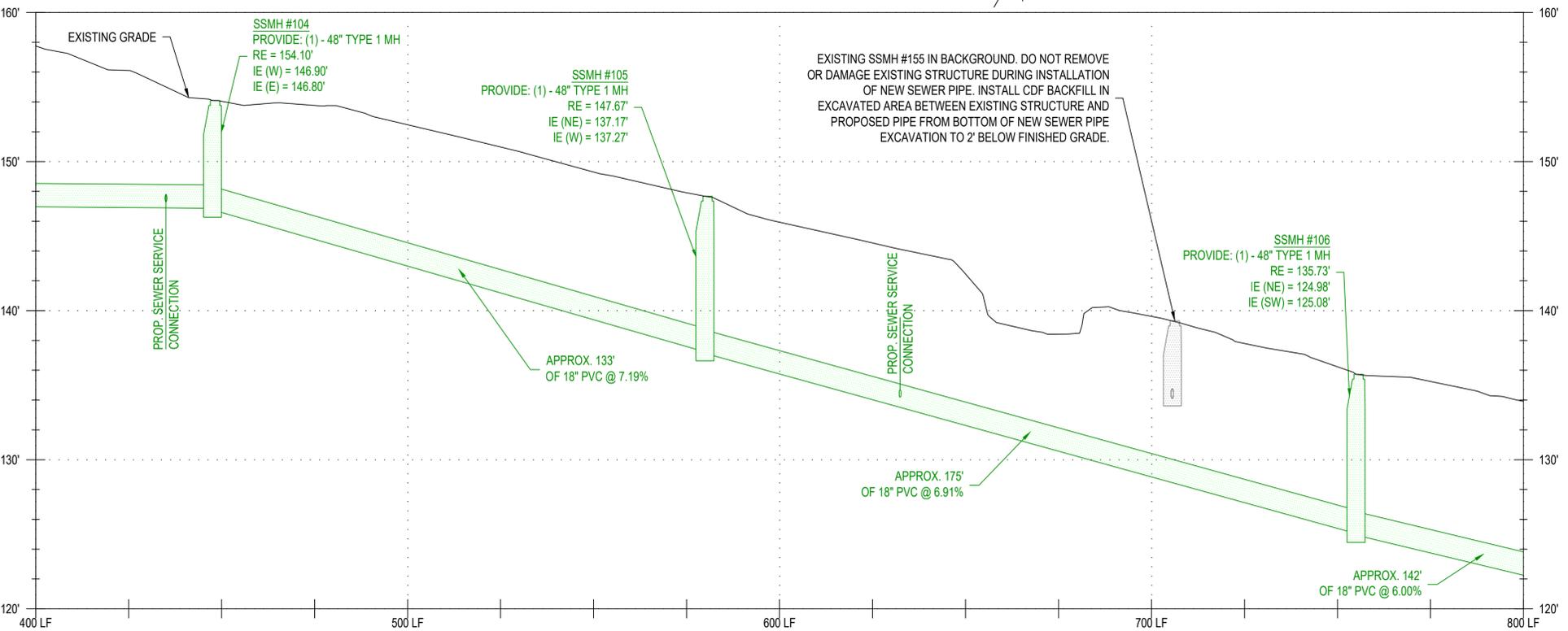
SCALE: SHOWN

DRAWING IS FULL SCALE WHEN BAR MEASURES 2"

DWG NO.: SS01
SHEET NO.: 4
19



PLAN VIEW
1" = 20'



PROFILE VIEW
H: 1" = 20'; V: 1" = 5'

GENERAL NOTES

- ALL SIDE SEWERS SHALL BE CONSTRUCTED PER CITY STANDARD DETAILS. A MINIMUM SLOPE OF 2% IS REQUIRED. IF MINIMUM SLOPE CAN NOT BE ACHIEVED, THE CONTRACTOR SHALL COORDINATE WITH THE CITY ON A MINIMUM ACHIEVABLE SLOPE.
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- CONFIRM ELEVATIONS AT ALL EXISTING MANHOLES WITH PROPOSED CONNECTIONS PRIOR TO ORDERING PARTS. RIM ELEVATIONS AND INVERTS HAVE BEEN APPROXIMATED.
- SEE DWG. D03 FOR ROADWAY RESTORATION DETAILS.
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- ALL DISTURBED AREA SHALL BE RESTORED TO EXISTING OR BETTER CONDITIONS PER RESTORATION SHEETS.
- PROVIDE ETHAFOAM PAD EQUAL TO DOW ETHAFOAM 220 BETWEEN CROSSING UTILITIES WITH FEWER THAN 6" OF CLEARANCE. PAD SHALL FILL THE ENTIRE GAP.
- PROVIDE SITE SECURITY FOR OPEN EXCAVATIONS INCLUDING, BUT NOT LIMITED TO, CONSTRUCTION FENCING, STEEL PLATES, AND BARRICADES FOR OPEN EXCAVATIONS AT ALL TIMES WHEN CONTRACTOR IS NOT ON-SITE.
- ONCE BYPASS IS FUNCTIONAL AND APPROVED BY CITY, REMOVE EXISTING SEWER PIPE AS ENCOUNTERED WITHIN TRENCH. GROUT ABANDONED PIPE ENDS WITH PRECISION NON-SHRINK GROUT. GROUT PLUG LENGTH SHALL BE 2 TIMES PIPE DIAMETER.

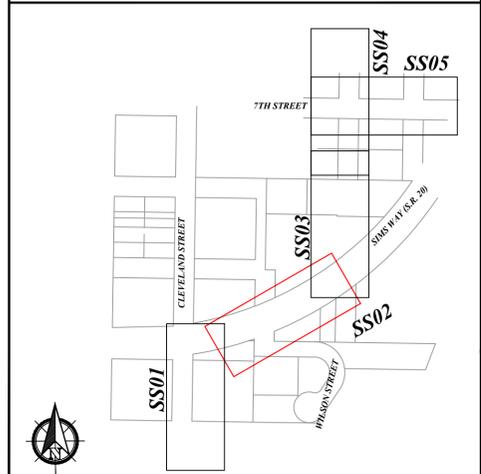
SIDE SEWER NOTES

- 3 SIDE SEWER RECONNECTION (SS-1A D02)
- 4 SIDE SEWER RECONNECTION (SS-1A D02)

KEY NOTES

- A** INSERT CATCH BASIN SILT SACK PER DETAIL ON DWG NO. D01. PROTECT CATCH BASIN AS NECESSARY DURING CONSTRUCTION OF PROPOSED IMPROVEMENTS. CLEAN OR REPLACE INLET PROTECTION DEVICES WHEN SEDIMENT HAS FILLED ONE-THIRD OF THE AVAILABLE STORAGE. IF CATCH BASIN IS DAMAGED DURING CONSTRUCTION, REMOVE AND REPLACE CATCH BASIN WITH AN APPROVED EQUAL.
- C** REMOVE AND REPLACE STORM CATCH BASIN AND STORM PIPE AS NECESSARY TO COMPLETE SEWER IMPROVEMENTS. IF ELEMENTS OF THE STORM SYSTEM ARE OUT OF SERVICE DURING CONSTRUCTION, INSTALL TEMPORARY STORMWATER BYPASS SYSTEM TO CONVEY STORMWATER TO NEAREST CATCH BASIN OR OTHER DISCHARGE POINT APPROVED BY THE CITY. IF REQUIRED, REPLACEMENT CATCH BASIN SHALL BE A WSDOT TYPE 1 CATCH BASIN. REPLACEMENT STORM PIPE SHALL MATCH EXISTING PIPE MATERIAL OR BE CPP IN COMPLIANCE WITH WSDOT STANDARD SPECIFICATIONS SECTION 9-05.1(7).
- D** ABANDON EXISTING SEWER STRUCTURE PER WSDOT STANDARD SPECIFICATION 7-05.3(2)
- E** REPLACE CONCRETE CULVERT IF IMPACTED DURING SEWER INSTALLATION OR ROAD RESTORATION. CULVERT SHALL MATCH EXISTING INVERTS. CONCRETE CULVERT MATERIAL SHALL BE PER WSDOT STANDARD SPECIFICATIONS SECTIONS 9-05.3(1) AND 9-05.3(1)A FOR PLAIN CONCRETE CULVERT PIPE.

KEY PLAN






CITY OF PORT TOWNSEND
HOLCOMB AND WILSON
SEWER REPLACEMENT

SEWER MAIN PLAN AND PROFILE 2

NO.	DATE	DESCRIPTION	BY	REVIEW

ENGINEER: CRB
 SW: DATE: Jan 23, 2026
 REVIEWED: RJF
 PLOT DATE: Jan 23, 2026

CLIENT: TOWNSEND
 FILENAME: HOL-D-SEWERP-NORTH.DWG
 JOB NO.: 25-0104

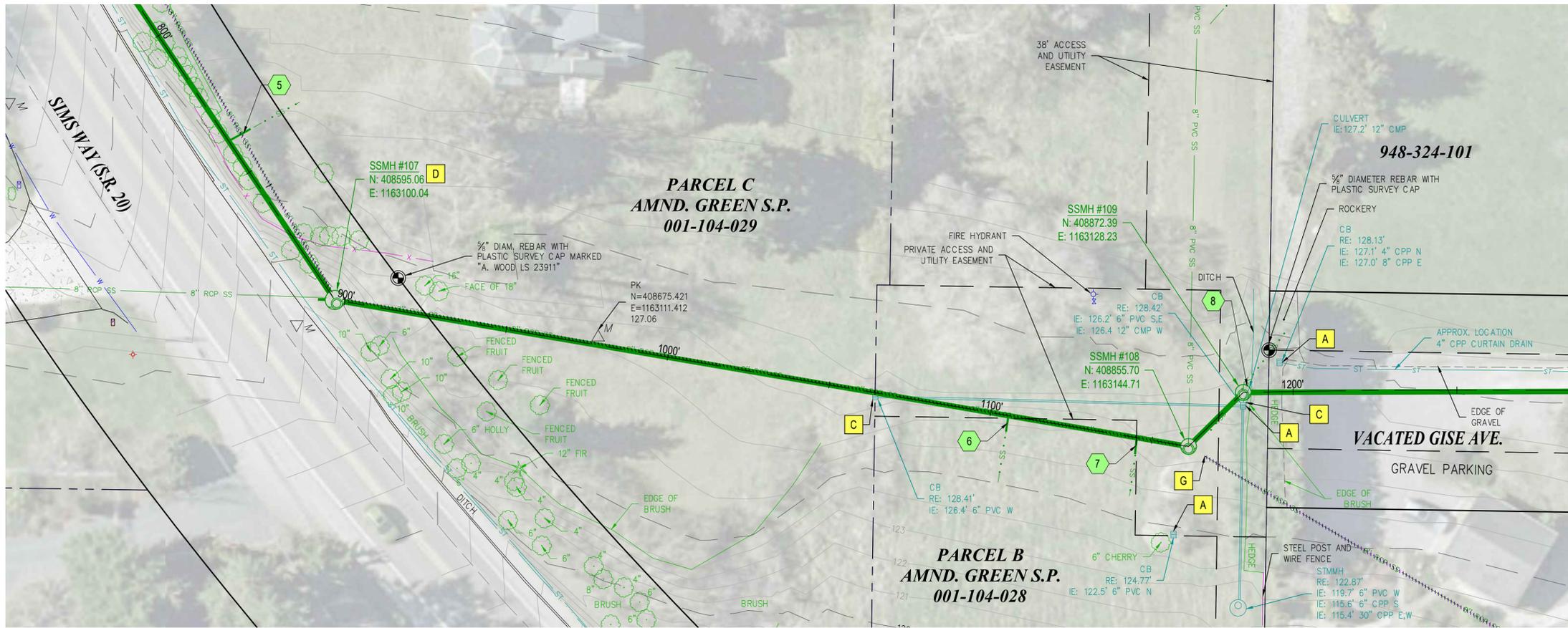
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DRAWING IS FULL SCALE WHEN BAR MEASURES 2"

DWG NO.: SS02

SHEET NO.: 5

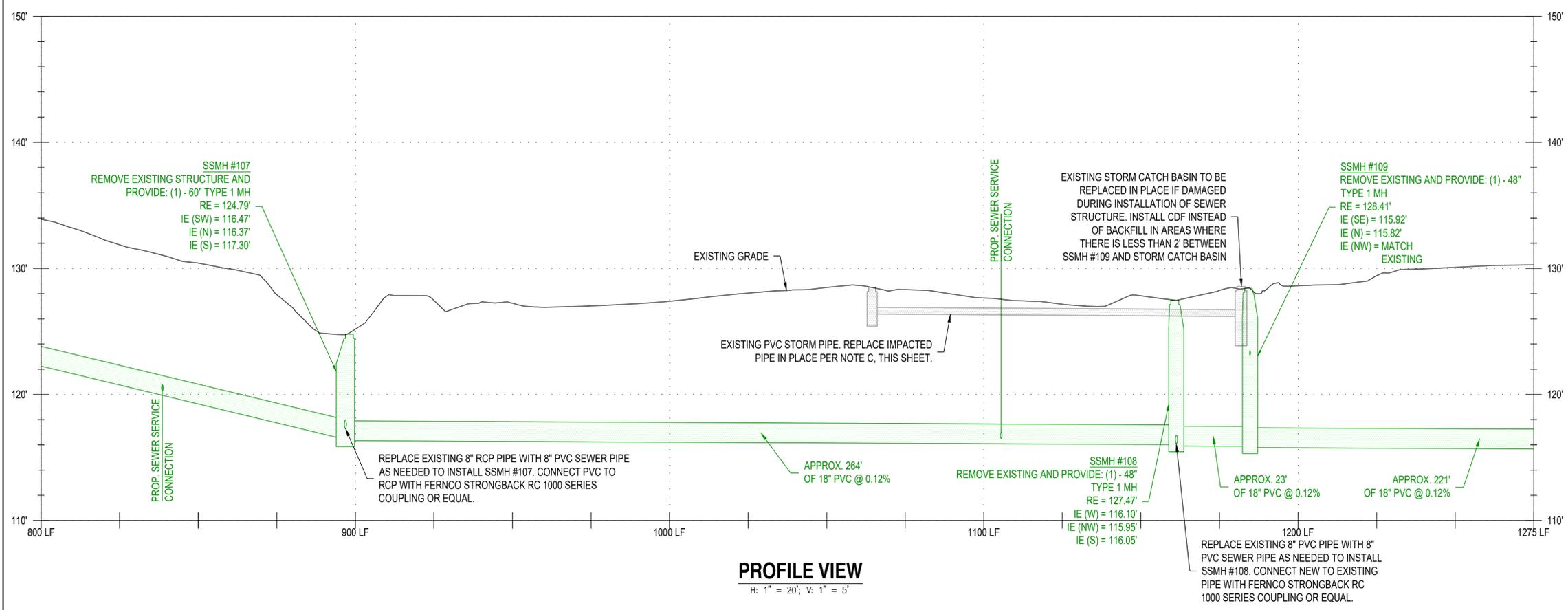
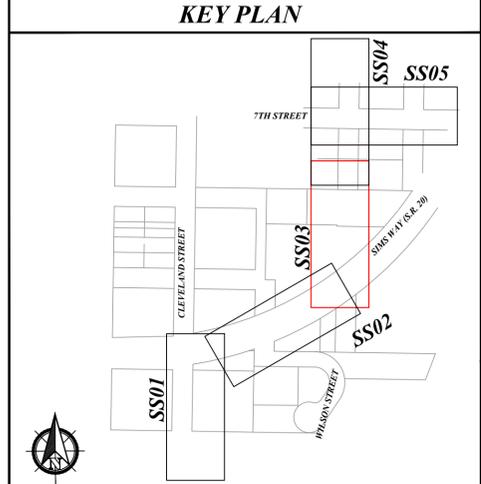
19



- ### GENERAL NOTES
- ALL SIDE SEWERS SHALL BE CONSTRUCTED PER CITY STANDARD DETAILS. A MINIMUM SLOPE OF 2% IS REQUIRED. IF MINIMUM SLOPE CAN NOT BE ACHIEVED, THE CONTRACTOR SHALL COORDINATE WITH THE CITY ON A MINIMUM ACHIEVABLE SLOPE.
 - EXISTING UTILITIES HAVE NOT BEEN POTHOLED TO IDENTIFY LOCATION, MATERIAL, SIZE, OR GEOMETRY. POTHOLE EXISTING UTILITY CROSSINGS TO VERIFY LOCATION, MATERIAL, SIZE, AND DEPTH PRIOR TO INSTALLING PROPOSED MAIN.
 - CONFIRM ELEVATIONS AT ALL EXISTING MANHOLES WITH PROPOSED CONNECTIONS PRIOR TO ORDERING PARTS. RIM ELEVATIONS AND INVERTS HAVE BEEN APPROXIMATED.
 - SEE DWG. D03 FOR ROADWAY RESTORATION DETAILS.
 - SEWER FLOW INTERRUPTIONS SHALL BE MANAGED BY TEMPORARY BYPASSES OR OTHER APPROVED ALTERNATIVE.
 - ALL SIDE SEWER CONNECTIONS HAVE BEEN APPROXIMATED TO INTERCEPT THE SIDE SEWER FROM EACH HOME. THE CONTRACTOR SHALL VERIFY THE LOCATION AND DEPTH OF THE SIDE SEWER BY POTHOLES. THE CONTRACTOR SHALL COORDINATE WITH THE CITY IF THE APPROXIMATE CONNECTION POINT IS NOT ACHIEVABLE.
 - ALL DISTURBED AREA SHALL BE RESTORED TO EXISTING OR BETTER CONDITIONS PER RESTORATION SHEETS.
 - PROVIDE ETHAFOAM PAD EQUAL TO DOW ETHAFOAM 220 BETWEEN CROSSING UTILITIES WITH FEWER THAN 6" OF CLEARANCE. PAD SHALL FILL THE ENTIRE GAP.
 - PROVIDE SITE SECURITY FOR OPEN EXCAVATIONS INCLUDING, BUT NOT LIMITED TO, CONSTRUCTION FENCING, STEEL PLATES, AND BARRICADES FOR OPEN EXCAVATIONS AT ALL TIMES WHEN CONTRACTOR IS NOT ON-SITE.
 - ONCE BYPASS IS FUNCTIONAL AND APPROVED BY CITY, REMOVE EXISTING SEWER PIPE AS ENCOUNTERED WITHIN TRENCH. GROUT ABANDONED PIPE ENDS WITH PRECISION NON-SHRINK GROUT. GROUT PLUG LENGTH SHALL BE 2 TIMES PIPE DIAMETER.

- ### SIDE SEWER NOTES
- 5 SIDE SEWER RECONNECTION (SS-1A D02)
 - 6 SIDE SEWER RECONNECTION (SS-1A D02)
 - 7 SIDE SEWER RECONNECTION (SS-1A D02)
 - 8 SIDE SEWER RECONNECTION (SS-1A D02)

- ### KEY NOTES
- A** INSERT CATCH BASIN SILT SACK PER DETAIL ON DWG NO. D01. PROTECT CATCH BASIN AS NECESSARY DURING CONSTRUCTION OF PROPOSED IMPROVEMENTS. CLEAN OR REPLACE INLET PROTECTION DEVICES WHEN SEDIMENT HAS FILLED ONE-THIRD OF THE AVAILABLE STORAGE. IF CATCH BASIN IS DAMAGED DURING CONSTRUCTION, REMOVE AND REPLACE CATCH BASIN WITH AN APPROVED EQUAL.
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 - D** ABANDON EXISTING SEWER STRUCTURE PER WSDOT STANDARD SPECIFICATION 7-05.3(2)
 - G** REMOVE EXISTING PIPE WITHIN TRENCH LIMITS. GROUT PIPE END WITH NON-SHRINK GROUT EQUAL TO PRECISION NON-SHRINK GROUT. EXISTING PIPE SHALL BE PLUGGED FOR A LENGTH OF 2 PIPE DIAMETERS.



CITY OF PORT TOWNSEND
HOLCOMB AND WILSON
SEWER REPLACEMENT

SEWER MAIN PLAN AND PROFILE 3

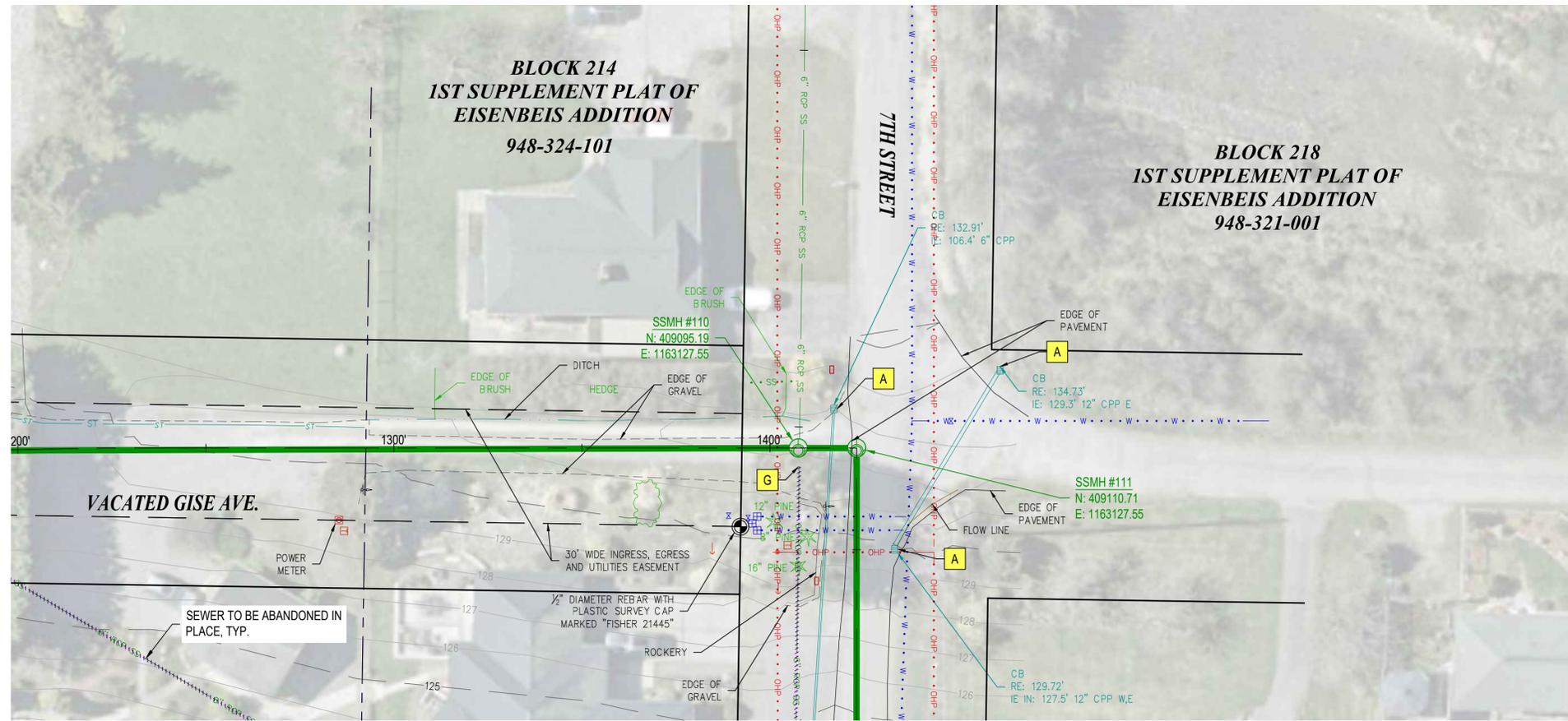
ENGINEER: CRB
DATE: Jan 23, 2026
PROJECT: HOL-D-SEWERP-NORTH/DWG

REVIEWED: RJF
DATE: Jan 23, 2026

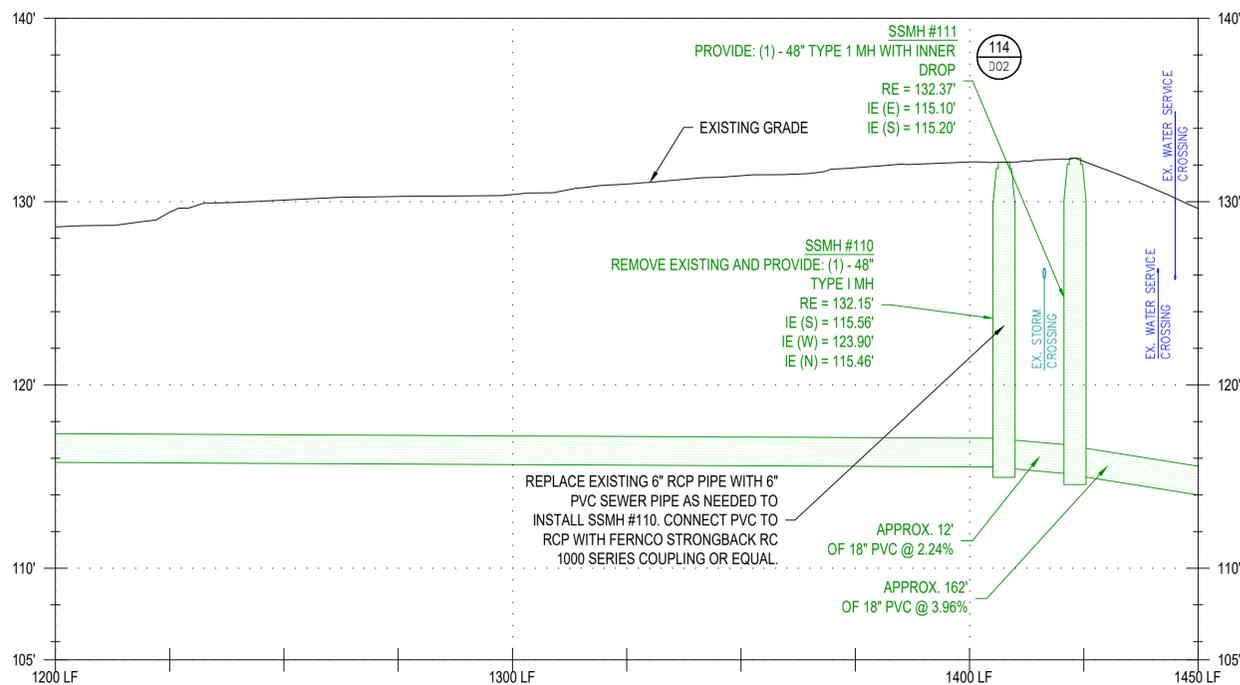
JOB NO.: 25-0104

SCALE: SHOWN

DWG NO.: SS03 SHEET NO.: 6 OF 19



PLAN VIEW
1" = 20'



PROFILE VIEW
H: 1" = 20'; V: 1" = 5'

GENERAL NOTES

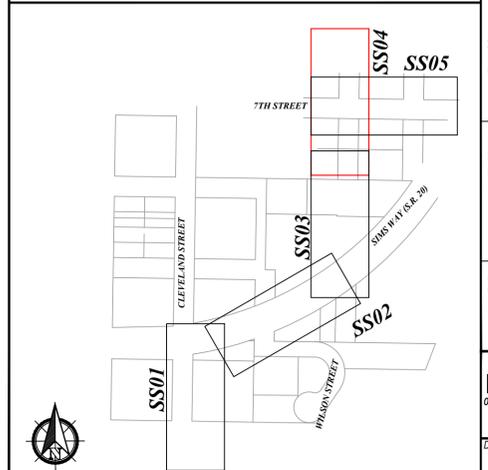
1. ALL SIDE SEWERS SHALL BE CONSTRUCTED PER CITY STANDARD DETAILS. A MINIMUM SLOPE OF 2% IS REQUIRED. IF MINIMUM SLOPE CAN NOT BE ACHIEVED, THE CONTRACTOR SHALL COORDINATE WITH THE CITY ON A MINIMUM ACHIEVABLE SLOPE.
2. EXISTING UTILITIES HAVE NOT BEEN POTHOLED TO IDENTIFY LOCATION, MATERIAL, SIZE, OR GEOMETRY. POTHOLE EXISTING UTILITY CROSSINGS TO VERIFY LOCATION, MATERIAL, SIZE, AND DEPTH PRIOR TO INSTALLING PROPOSED MAIN.
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4. SEE DWG. D03 FOR ROADWAY RESTORATION DETAILS.
5. SEWER FLOW INTERRUPTIONS SHALL BE MANAGED BY TEMPORARY BYPASSES OR OTHER APPROVED ALTERNATIVE.
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9. PROVIDE SITE SECURITY FOR OPEN EXCAVATIONS INCLUDING, BUT NOT LIMITED TO, CONSTRUCTION FENCING, STEEL PLATES, AND BARRICADES FOR OPEN EXCAVATIONS AT ALL TIMES WHEN CONTRACTOR IS NOT ON-SITE.
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SIDE SEWER NOTES

KEY NOTES

- A** INSERT CATCH BASIN SILT SACK PER DETAIL ON DWG NO. D01. PROTECT CATCH BASIN AS NECESSARY DURING CONSTRUCTION OF PROPOSED IMPROVEMENTS. CLEAN OR REPLACE INLET PROTECTION DEVICES WHEN SEDIMENT HAS FILLED ONE-THIRD OF THE AVAILABLE STORAGE. IF CATCH BASIN IS DAMAGED DURING CONSTRUCTION, REMOVE AND REPLACE CATCH BASIN WITH AN APPROVED EQUAL.
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KEY PLAN



RH2

PRELIMINARY

PROFESSIONAL ENGINEER

CITY OF PORT TOWNSEND
HOLCOMB AND WILSON
SEWER REPLACEMENT

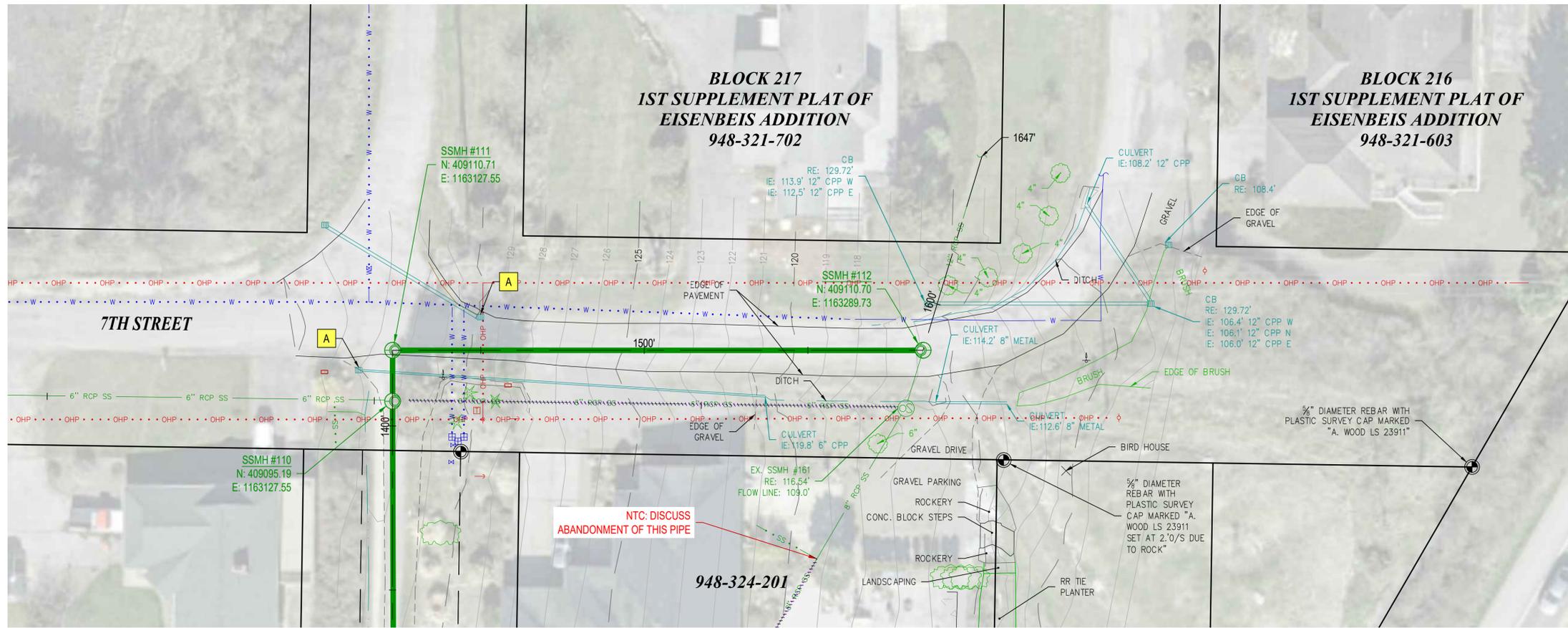
SEWER MAIN PLAN AND PROFILE 4

NO.	DATE	DESCRIPTION	BY	REVIEW

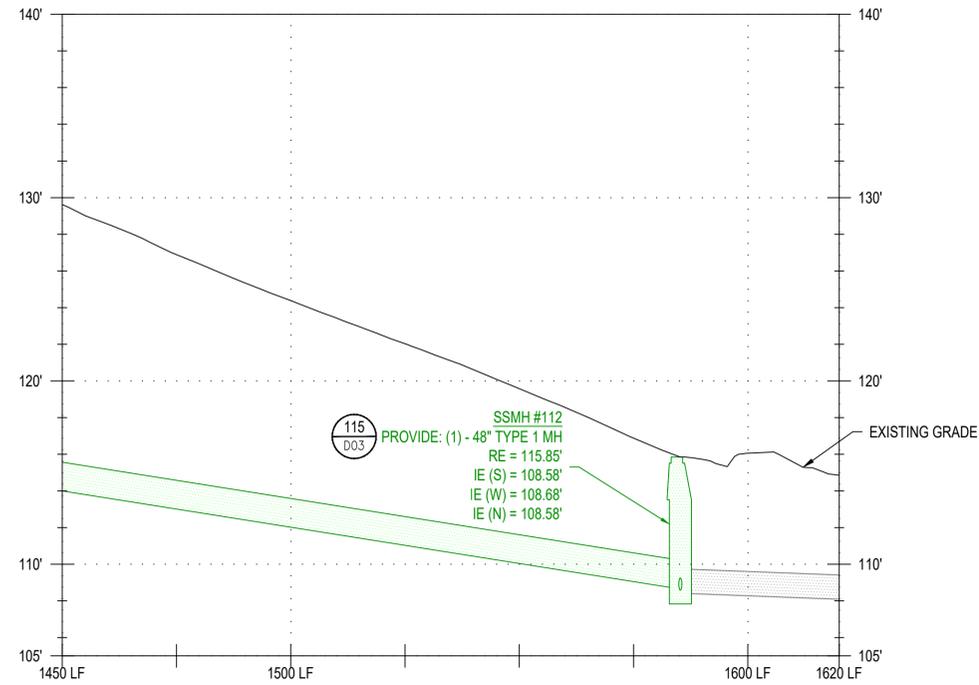
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DRAWING IS FULL SCALE WHEN BAR MEASURES 2"

DWG NO.: **SS04** SHEET NO.: **7** OF **19**



PLAN VIEW
1" = 20'



PROFILE VIEW
H: 1" = 20'; V: 1" = 5'

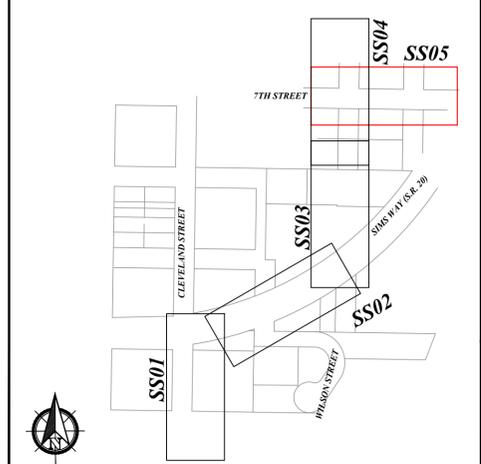
GENERAL NOTES

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KEY PLAN



RH2

PRELIMINARY

PROFESSIONAL ENGINEER

CITY OF PORT TOWNSEND
HOLCOMB AND WILSON
SEWER REPLACEMENT

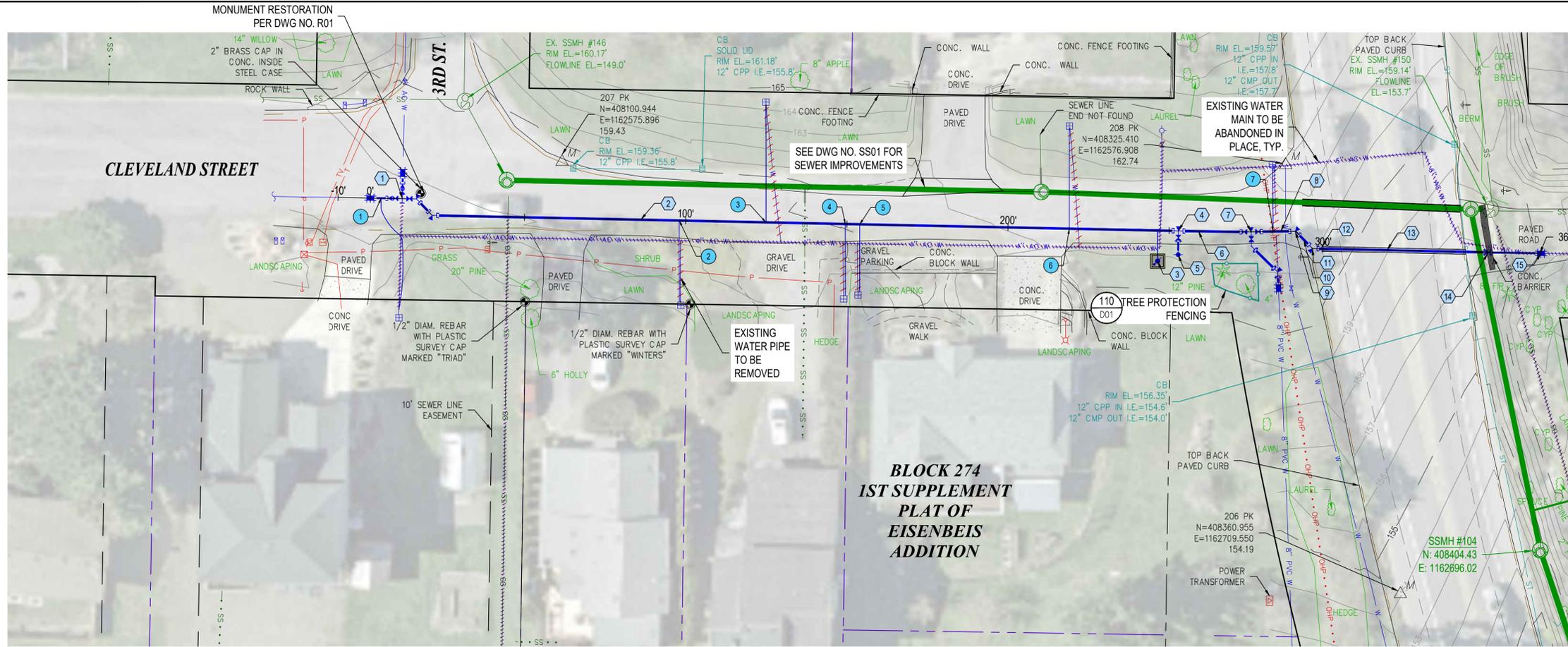
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NO.	DATE	DESCRIPTION	BY	REVIEW

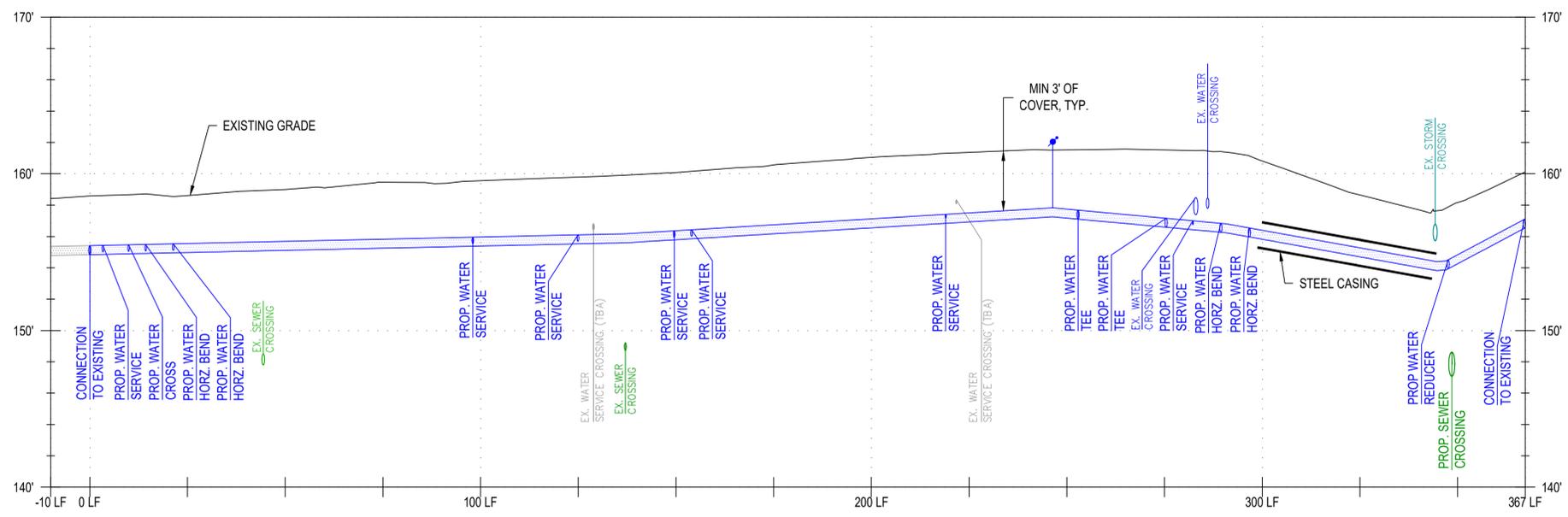
SCALE: SHOWN

DRAWING IS FULL SCALE WHEN BAR MEASURES 2"

DWG NO.: **SS05** SHEET NO.: **8** OF **19**



PLAN VIEW
1" = 20'



PROFILE VIEW
H: 1" = 20'; V: 1" = 5'

GENERAL NOTES

- VEGETATION AND PRIVATE PROPERTY RESTORATION LIMITS ARE NOT SHOWN FOR CLARIFICATION. RESTORE ALL DISTURBED VEGETATION AND PRIVATE PROPERTY TO EXISTING OR BETTER CONDITIONS TO THE SATISFACTION OF THE PROPERTY OWNER AND/OR CITY REPRESENTATIVE.
- THE DISPLAYED LOCATION AND DEPTH OF ALL EXISTING UTILITIES ARE ASSUMED. POTHOLE ALL EXISTING CROSSING UTILITIES A MINIMUM OF 72 HOURS IN ADVANCE OF PIPE OR STRUCTURE INSTALLATION. AT CONNECTION TO EXISTING WATER MAIN LOCATIONS, POTHOLE EXISTING WATER MAIN PRIOR TO INSTALLATION OF PIPE WITHIN 100 FT OF CONNECTION POINT.

- WATER MAIN NOTES**
- 1 N: 408051.48 E: 1162587.26, PROVIDE: (1) - CONNECTION TO EXISTING (A) (B) (D04) (D04)
 - 2 INSTALL APPROX 229 LF OF 8" DI PIPE
 - 3 N: 408285.74 E: 1162606.61, PROVIDE: (1) - 2" COMBINATION AIR RELEASE/VACUUM VALVE ASSEMBLY (2-7) (D03)
 - 4 N: 408292.2 E: 1162597.26, PROVIDE: (1) - FIRE HYDRANT ASSEMBLY (2-12) (D04)
 - 5 N: 408292.11 E: 1162604.62, PROVIDE: LOCATION OF CENTER OF HYDRANT
 - 6 INSTALL APPROX 17 LF OF 8" DI PIPE
 - 7 N: 408314.75 E: 1162597.54, PROVIDE: (1) - CONNECTION TO EXISTING (C) (D04)
 - 8 INSTALL APPROX 12 LF OF 8" DI PIPE
 - 9 N: 408328.42 E: 1162597.35, PROVIDE: (1) - 8" DI 45° BEND (MxM) WITH CONCRETE THRUST BLOCKING
 - 10 INSTALL APPROX 8 LF OF 8" DI PIPE
 - 11 N: 408333.78 E: 1162602.73, PROVIDE: (1) - 8" DI 45° BEND (MxM) WITH CONCRETE THRUST BLOCKING
 - 12 N: 408336.66 E: 1162602.77, PROVIDE: (1) - MIN. 16" OD STEEL CASING PIPE, APPROX. 45 LF (109) (D02)
 - 13 INSTALL APPROX 45 LF OF 8" DI PIPE
 - 14 N: 408387.44 E: 1162603.89, PROVIDE: (1) - 8" x 6" DI REDUCER (RjxRj)
 - 15 N: 408404.63 E: 1162604.04, PROVIDE: (1) - CONNECTION TO EXISTING (D) (D04)

- WATER SERVICE NOTES**
- 1 N: 408044.91 E: 1162587.15, PROVIDE: (1) - WATER SERVICE CONNECTION
 - 2 N: 408137.38 E: 1162594.07, PROVIDE: (1) - WATER SERVICE CONNECTION
 - 3 N: 408164.27 E: 1162594.63, PROVIDE: (1) - WATER SERVICE CONNECTION
 - 4 N: 408188.89 E: 1162595.15, PROVIDE: (1) - WATER SERVICE CONNECTION
 - 5 N: 408193.34 E: 1162595.24, PROVIDE: (1) - WATER SERVICE CONNECTION
 - 6 N: 408258.35 E: 1162596.61, PROVIDE: (1) - WATER SERVICE CONNECTION
 - 7 N: 408316.56 E: 1162596.63, PROVIDE: (1) - WATER SERVICE CONNECTION



CITY OF PORT TOWNSEND
 HOLCOMB AND WILSON
 SEWER REPLACEMENT

WATER MAIN PLAN AND PROFILE

ENGINEER: CRB
 REVIEWED: RJF

SWF DATE: Jan 22, 2026
 PLOT DATE: Jan 23, 2026

CLIENT: TOWNSEND
 FILENAME: HOL-D-WATERP-NORTH.DWG

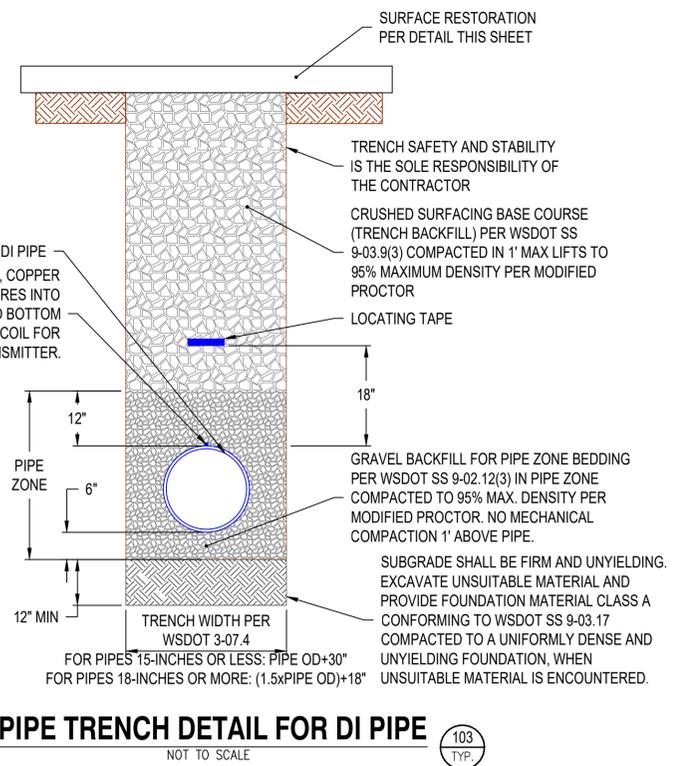
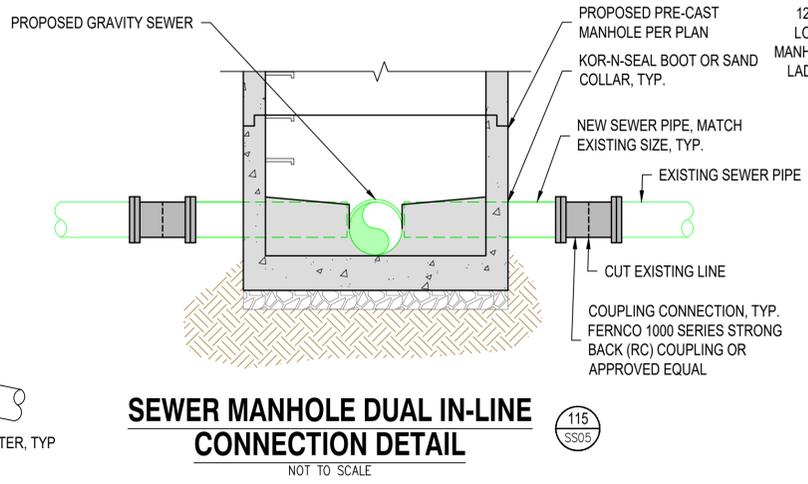
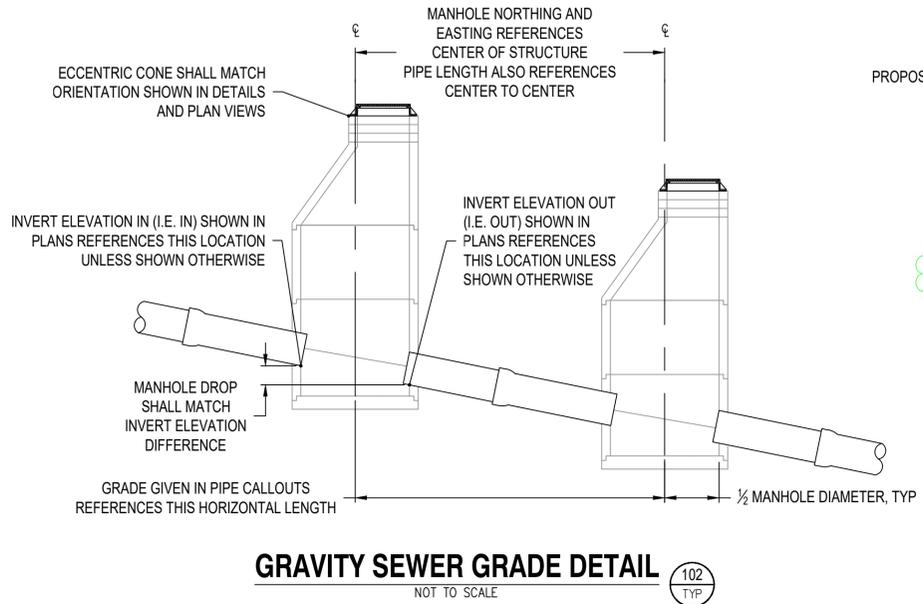
JOB NO.: 25-0104

REVISIONS			
NO.	DATE	DESCRIPTION	BY

SCALE: SHOWN
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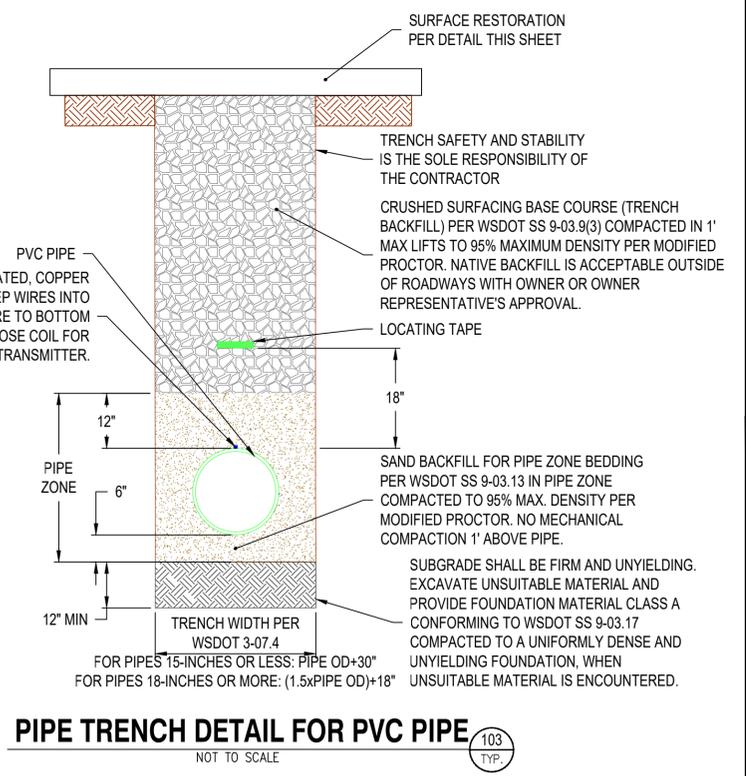
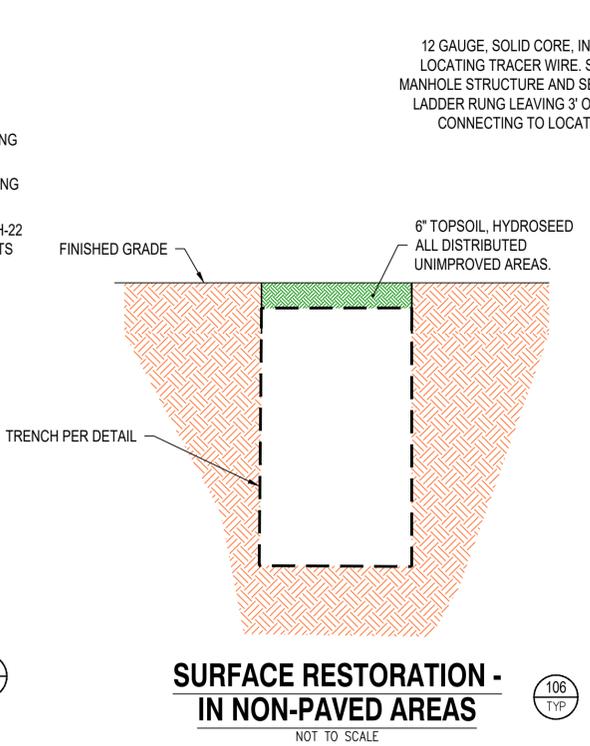
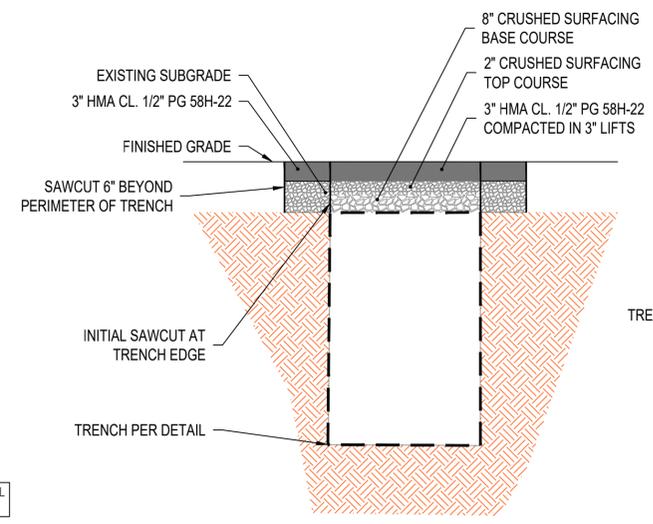
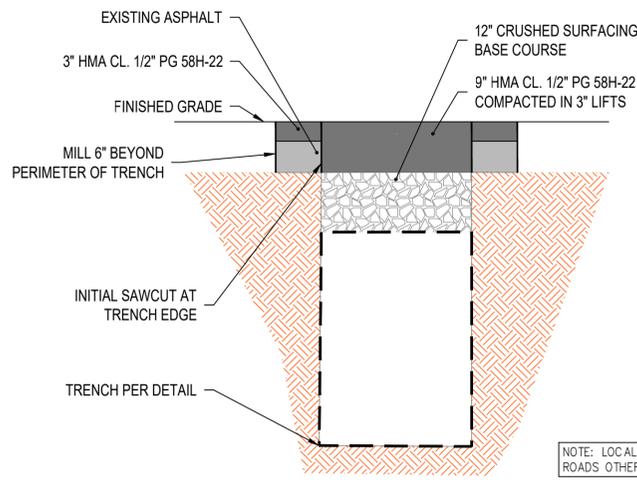
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 SHEET NO.: 9
 OF 19

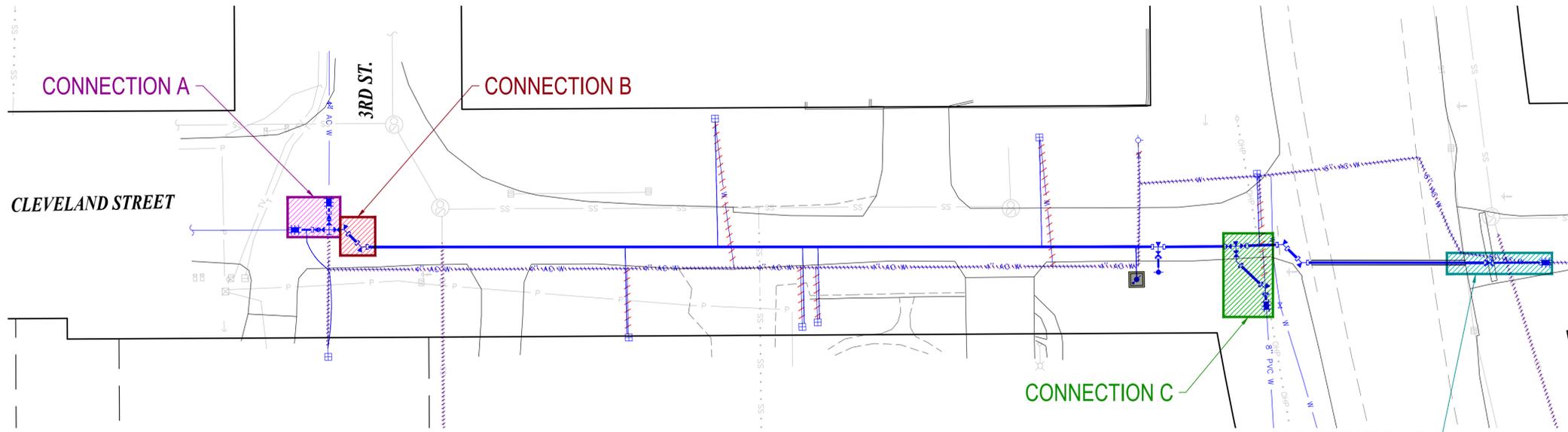
NO.	DATE	DESCRIPTION	BY	REVIEW



SURFACE RESTORATION NOTES

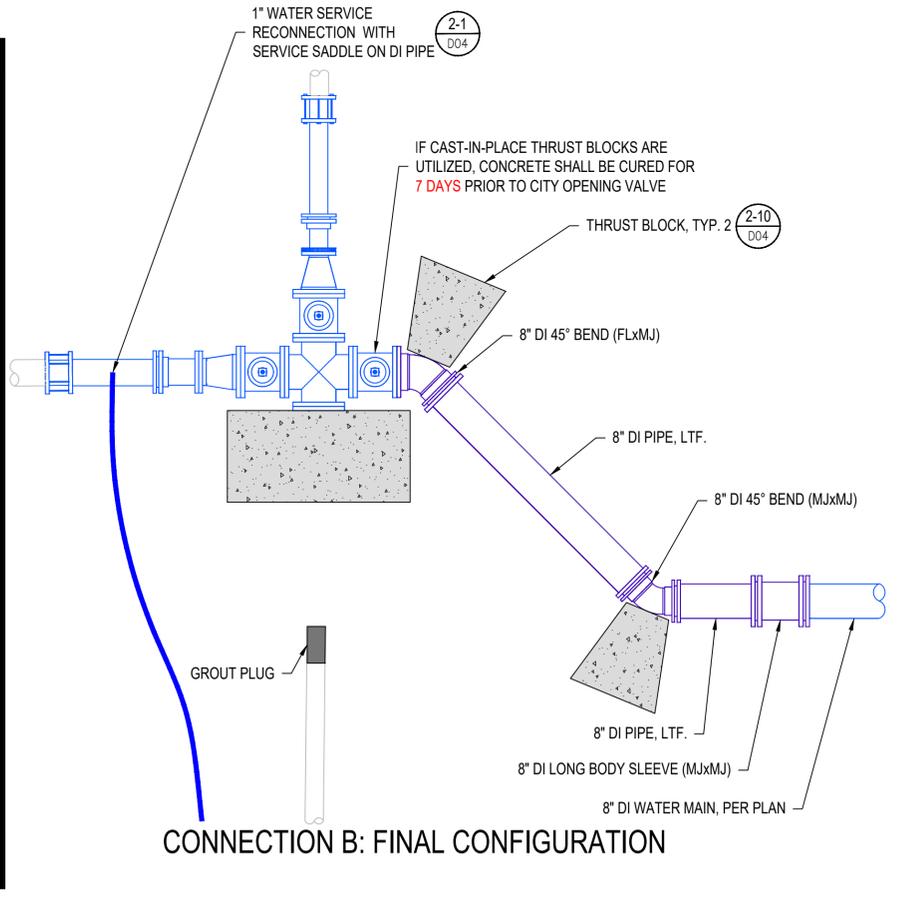
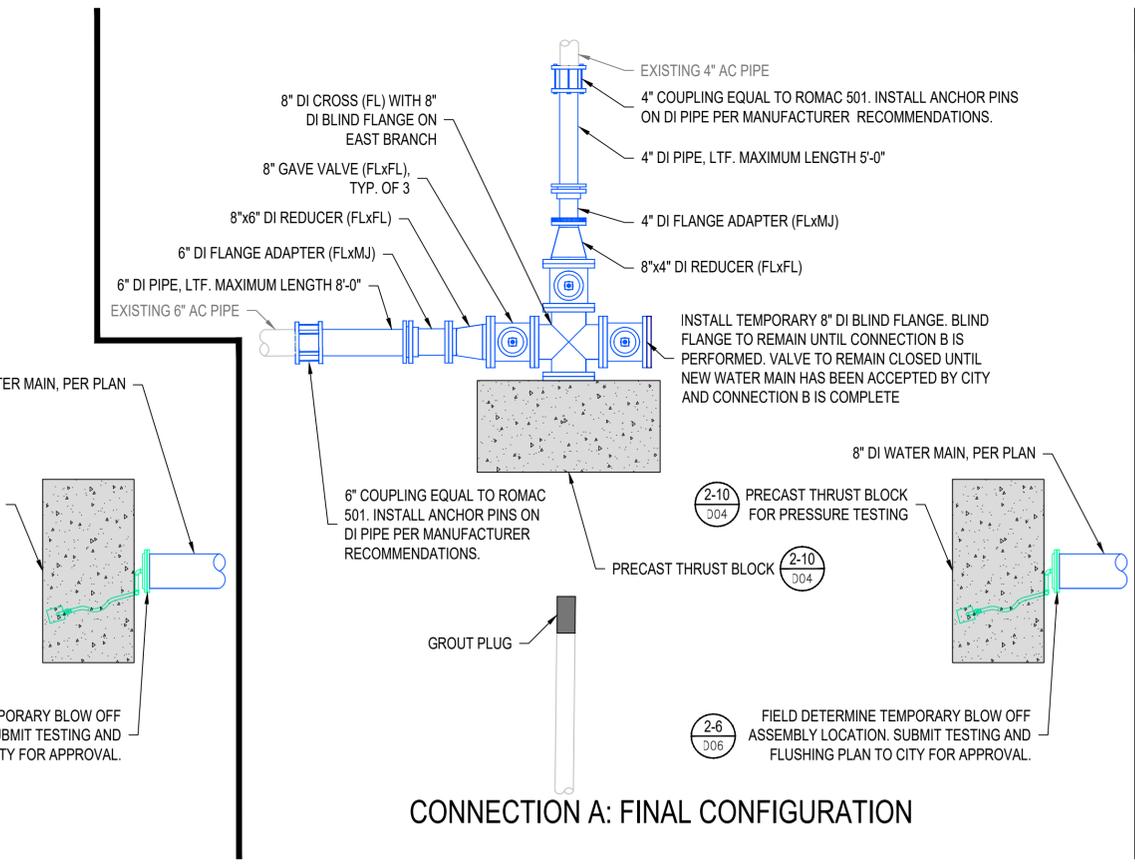
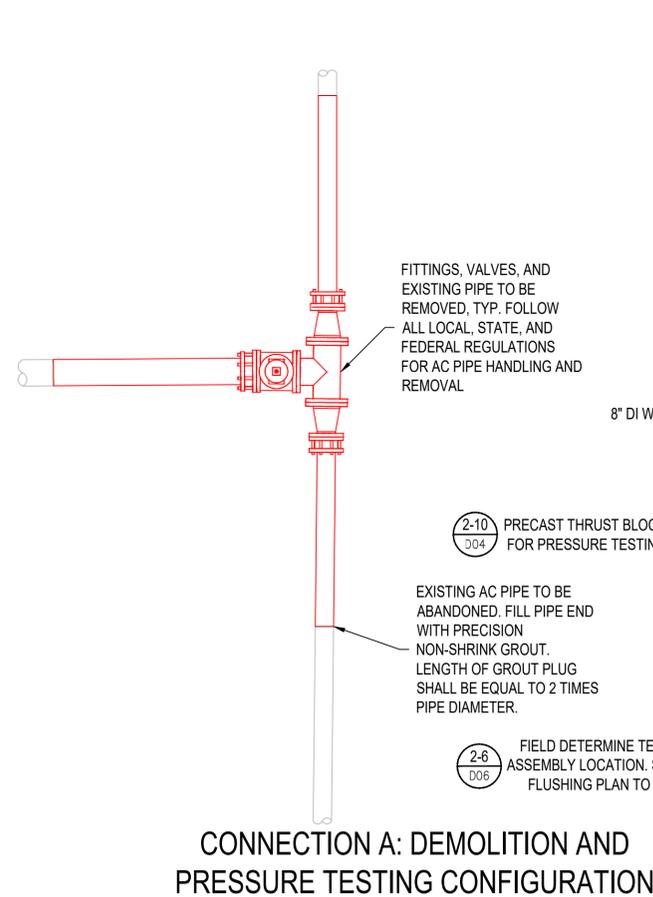
- TRENCH PATCHES SHALL NOT BE HIGHER THAN, AND NO MORE THAN 1/8 (ONE-EIGHTH) INCH LOWER THAN, THE SURROUNDING EXISTING ASPHALT.
- SAWCUT PRIOR TO FINAL PATCHING.
- SHOULDER SHALL BE RESTORED TO EXISTING OR BETTER CONDITION.



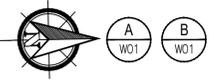


- ### WATER MAIN PHASING PLAN
1. MAKE CONNECTION A. COORDINATE WITH CITY TO SHUT OFF VALVES OFF CLEVELAND AND 3RD AVE. PROVIDE CITY WITH WRITTEN NOTICE AT LEAST ONE WEEK PRIOR TO CONNECTION. PROVIDE CUSTOMERS TO BE IMPACTED BY OUTAGES PROPER NOTICE PER PROJECT SPECIFICATIONS. MAXIMUM CUSTOMER OUTAGE SHALL BE ONE 8-HOUR PERIOD.
 2. INSTALL 8" DI WATER MAIN FROM SLEEVE ON SOUTH SIDE OF CLEVELAND STREET TO LOCATION OF REDUCER ON NORTH SIDE OF SIMS WAY PER DWG NO. W01. DO NOT INSTALL REDUCER AT THIS TIME. INSTALL TEMPORARY RJ CAP ON PIPE END PER CONNECTION DETAIL D.
 3. PRESSURE TEST, FLUSH, AND OBTAIN PASSING PURITY TEST ON NEW 8" DI WATER MAIN FROM SLEEVE ON SOUTH SIDE OF CLEVELAND STREET TO RJ CAP ON NORTH SIDE OF SIMS WAY. CONNECTION C IN-LINE VALVES TO REMAIN OPEN. CONNECTION C BRANCH VALVE TO REMAIN CLOSED.
 4. MAKE CONNECTION B.
 5. MAKE CONNECTION C. CLOSE VALVE ON NORTH SIDE OF NEW WATER MAIN TEE PER CONNECTION DETAIL.
 6. TRANSFER SERVICES FROM EXISTING WATER MAIN TO NEW WATER MAIN.
 7. MAKE CONNECTION D PER CONNECTION DETAIL. CITY TO OPEN VALVE ON NORTH SIDE OF CONNECTION C TEE.

WATER MAIN PHASING PLAN
1" = 20'



CONNECTIONS A AND B: CONNECTION TO EXISTING 4" AC MAIN ON SOUTH END OF CLEVELAND STREET
NOT TO SCALE



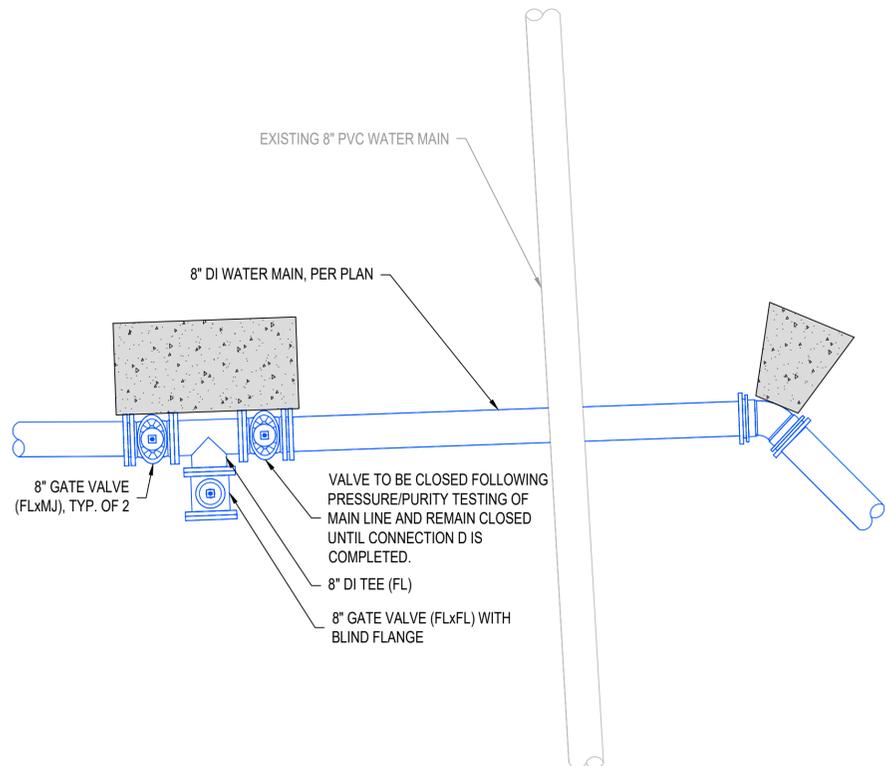
CITY OF PORT TOWNSEND
HOLCOMB AND WILSON
SEWER REPLACEMENT



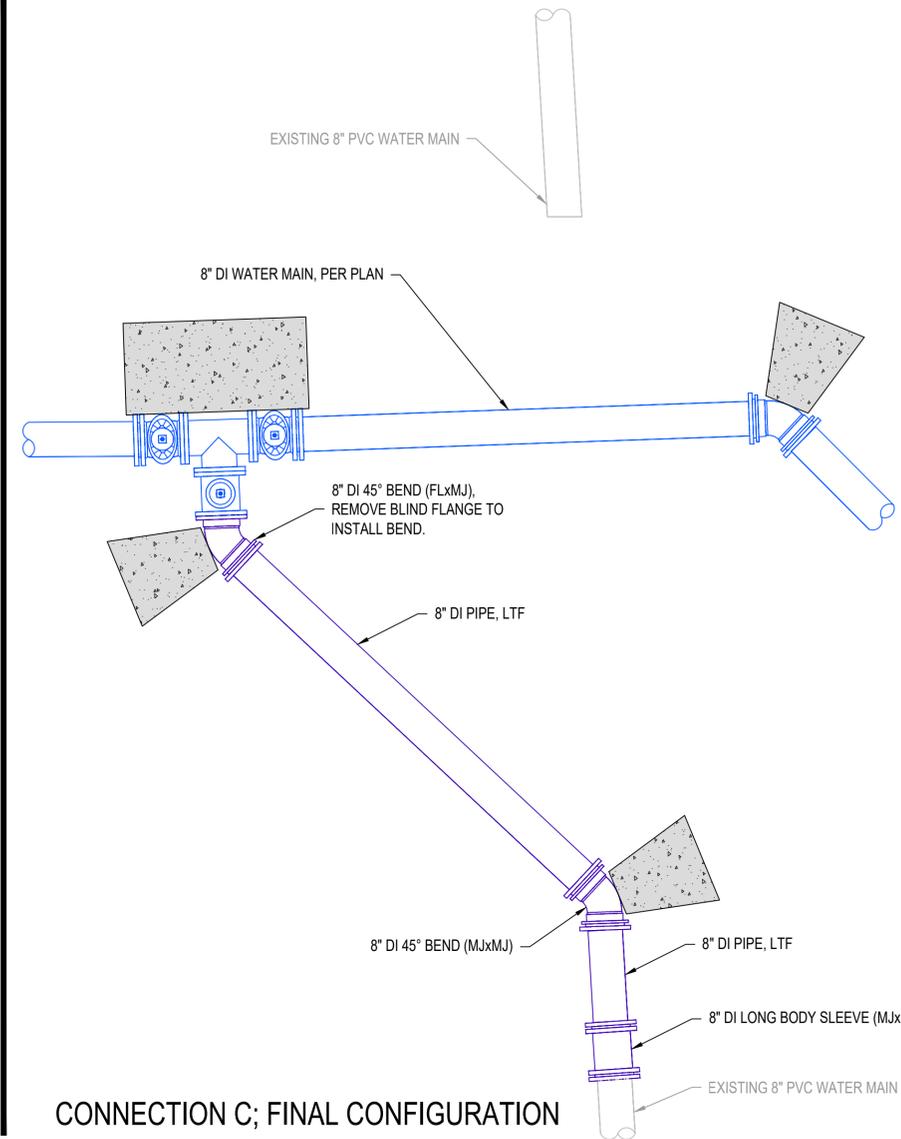
WATER MAIN PHASING PLAN AND CONNECTION DETAILS 1

NO.	DATE	DESCRIPTION	BY	REVIEW

ENGINEER: CRB	DATE: Jan 23, 2026	CLIENT: TOWNSEND	JOB NO.: 25-0104
REVIEWED: RJF	PROJECT DATE: Jan 23, 2026	FILENAME: HOL-D-DET01.DWG	
SCALE: SHOWN		DRAWING IS FULL SCALE WHEN BAR MEASURES 2"	
DWG NO.: D05	SHEET NO.: 13	19	



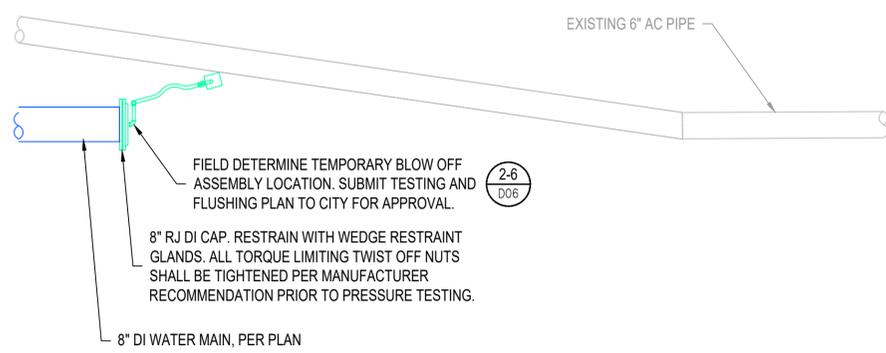
CONNECTION C: TESTING CONFIGURATION



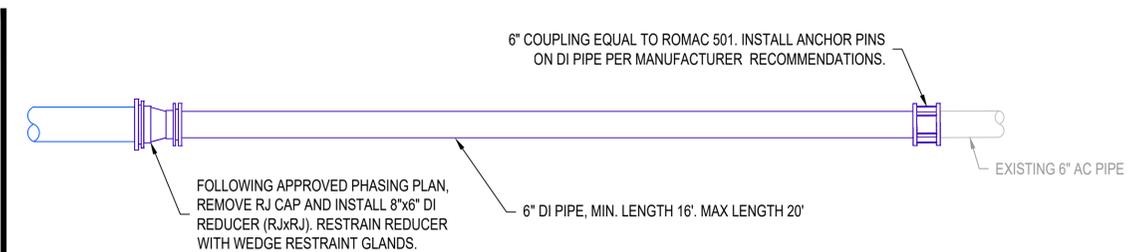
CONNECTION C; FINAL CONFIGURATION

CONNECTION C: CONNECTION TO EXISTING 8" PVC MAIN ON SOUTH SIDE OF SIMS WAY

NOT TO SCALE



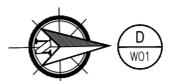
CONNECTION D: TESTING CONFIGURATION



CONNECTION D: FINAL CONFIGURATION

CONNECTION D: CONNECTION TO EXISTING 6" AC MAIN ON NORTH SIDE OF SIMS WAY

NOT TO SCALE



APPROVED BY: STEVE KING, PE
REVISOR: PUBLIC WORKS DIRECTOR
REVISION DATE: 2/1/2022
CITY OF PORT TOWNSEND PUBLIC WORKS
CONSTRUCTION/TEMPORARY BLOW-OFF ASSEMBLY
Std. Dwg. No.: 2-9

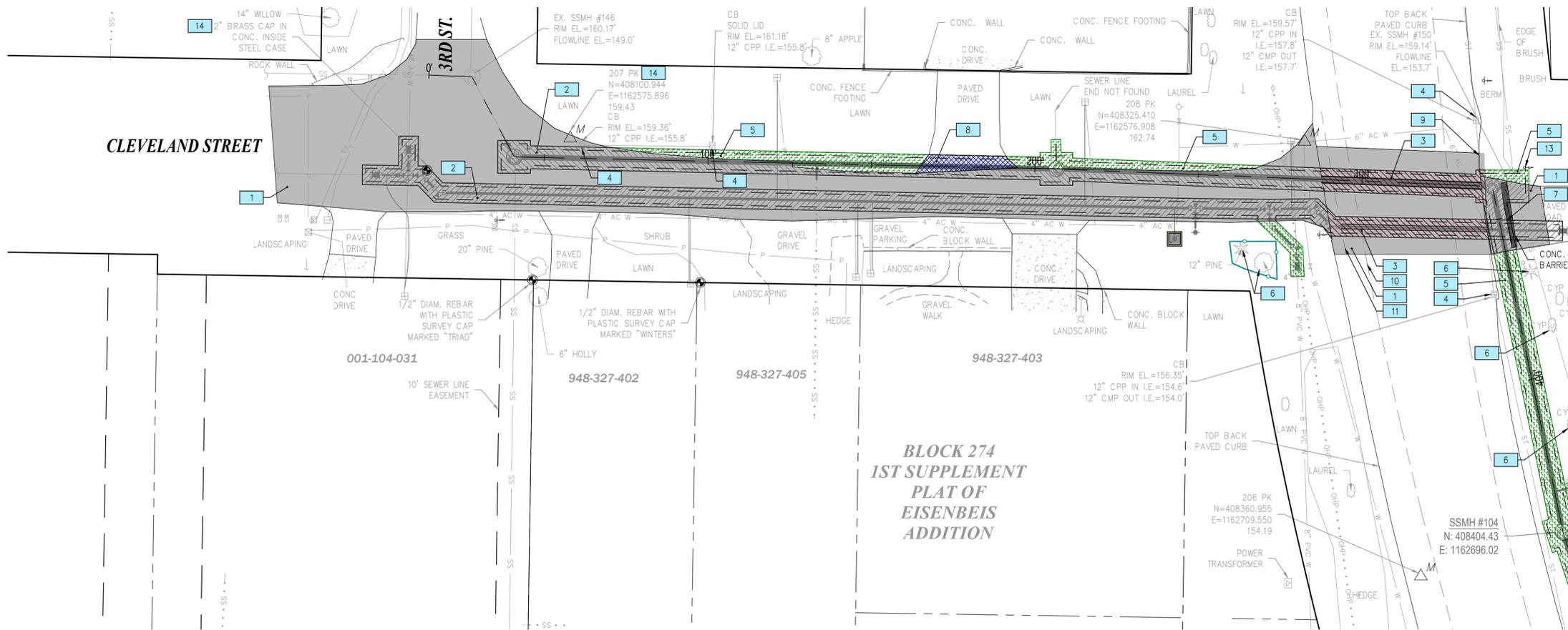


CITY OF PORT TOWNSEND
HOLCOMB AND WILSON
SEWER REPLACEMENT

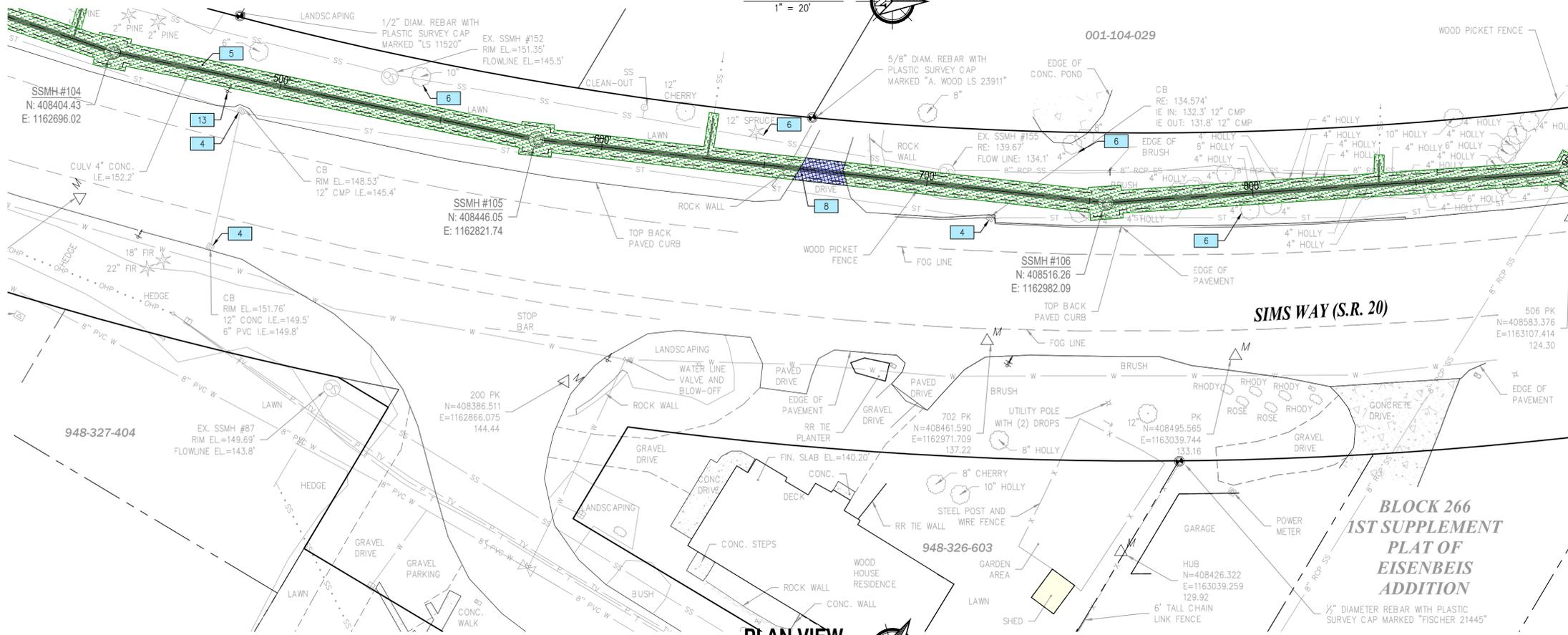
WATER MAIN CONNECTION DETAILS 2

NO.	DATE	DESCRIPTION	BY	REVIEW

SCALE: SHOWN
 0' 1' 2'
 DRAWING IS FULL SCALE WHEN BAR MEASURES 2"
 DWG NO.: D06 SHEET NO.: 14 OF 19



PLAN VIEW
1" = 20'



PLAN VIEW
1" = 20'

RESTORATION NOTES

ALL RESTORATION LIMITS SHOWN ARE APPROXIMATE. ANY CONCRETE RESTORATION REQUIRED SHALL EXTEND TO THE NEAREST JOINT OR EDGE.

- APPROX. TRENCH LIMITS
- 3-INCH ASPHALT GRIND AND OVERLAY
- ASPHALT TRENCH PATCH OUTSIDE OF SIMS WAY
- ASPHALT TRENCH PATCH WITHIN SIMS WAY
- CEMENT CONCRETE SIDEWALK
- CEMENT CONCRETE CURB AND GUTTER
- CEMENT CONCRETE DRIVEWAY
- RESTORATION WITHIN NON-PAVED AREAS
- ASPHALT REPAIR IN DRIVEWAY/PARKING LOT
- GRAVEL

- 1 FULL ROAD OVERLAY. PROPOSED WORK, INCLUDING GRIND AND OVERLAY SHALL BE AS SHOWN ON THE PLANS, OR AS REQUIRED BY THE CITY.
- 2 TRENCH PATCH SURFACE RESTORATION PER SURFACE RESTORATION DETAIL FOR LOCAL ROADS ON DWG NO. D03, TYP.
- 3 TRENCH PATCH SURFACE RESTORATION PER SURFACE RESTORATION DETAIL FOR W SIMS WAY ON DWG NO. D03, TYP.
- 4 INSERT CATCH BASIN SILT SACK PER DETAIL ON DWG NO. D01. PROTECT CATCH BASIN AS NECESSARY TO CONSTRUCT PROPOSED IMPROVEMENTS. IF CATCH BASIN IS DAMAGED DURING CONSTRUCTION, REMOVE AND REPLACE CATCH BASIN WITH AN APPROVED EQUAL. PROVIDE NEW CATCH BASIN AND RISERS AS NECESSARY TO MATCH RIM AND INVERT ELEVATIONS
- 5 RESTORE DISTURBED SURFACE TO EXISTING OR BETTER CONDITION PER SURFACE RESTORATION IN NON-PAVED AREA DETAIL ON DWG NO. D03, TYP.
- 6 PROTECT TREES AS NECESSARY TO AVOID DAMAGE WHILE CONSTRUCTING PROPOSED IMPROVEMENTS PER TREE PROTECTION NOTES ON DWG NO. D01. IF CITY OR ENGINEER DETERMINES THAT TREE OR TREE ROOTS WERE SUBSTANTIALLY DAMAGED DURING CONSTRUCTION, ENTIRE TREE INCLUDING ROOTS/ROOT BALL SHALL BE REMOVED AND REPLACED WITH TOPSOIL. NOT ALL TREES THAT MAY BE IMPACTED ARE SHOWN/LABELED ON THIS DRAWING.
- 7 REMOVE EXISTING CONCRETE BARRIER AND REPLACE WITH ONE PRECAST TYPE F CONCRETE BARRIER PER WSDOT STANDARD PLAN C-60. 10-04.
- 8 RESTORE EXISTING ASPHALT DRIVEWAY. ASPHALT SHALL HAVE A THICKNESS OF 4".
- 9 RESTORE EXISTING CURB AND GUTTER. MINIMUM RESTORATION SHALL BE TO THE NEAREST JOINT, TYP.
- 10 RESTORE ROADWAY PAINT AND STRIPING, INCLUDING ALL CENTERLINES, TURN LANES, AND FOG LINES, TO EXISTING OR BETTER CONDITIONS, TYP.
- 11 RESTORE ROADWAY MARKINGS TO EXISTING OR BETTER CONDITIONS, TYP.
- 12 RESTORE GRAVEL ROADWAY PER DETAIL FOR GRAVEL ROADS ON DWG NO. D03, TYP. ROAD GRADING SHALL MATCH EXISTING CONDITIONS.
- 13 IF DISTURBED OR DAMAGED, REPLACE SIGN.
- 14 PRESERVE MONUMENT DURING CONSTRUCTION. IF DISTURBED DURING CONSTRUCTION, MONUMENT SHALL BE REPLACED IN ACCORDANCE WITH RECOGNIZED SURVEYING PRACTICES BY A LAND SURVEYOR LICENSED IN WASHINGTON.
- 15 REINSTATE/REPLACE STORM CATCH BASIN AND STORM PIPE FOLLOWING CONSTRUCTION OF SEWER.

CITY OF PORT TOWNSEND
HOLCOMB AND WILSON
SEWER REPLACEMENT

RESTORATION PLAN 1

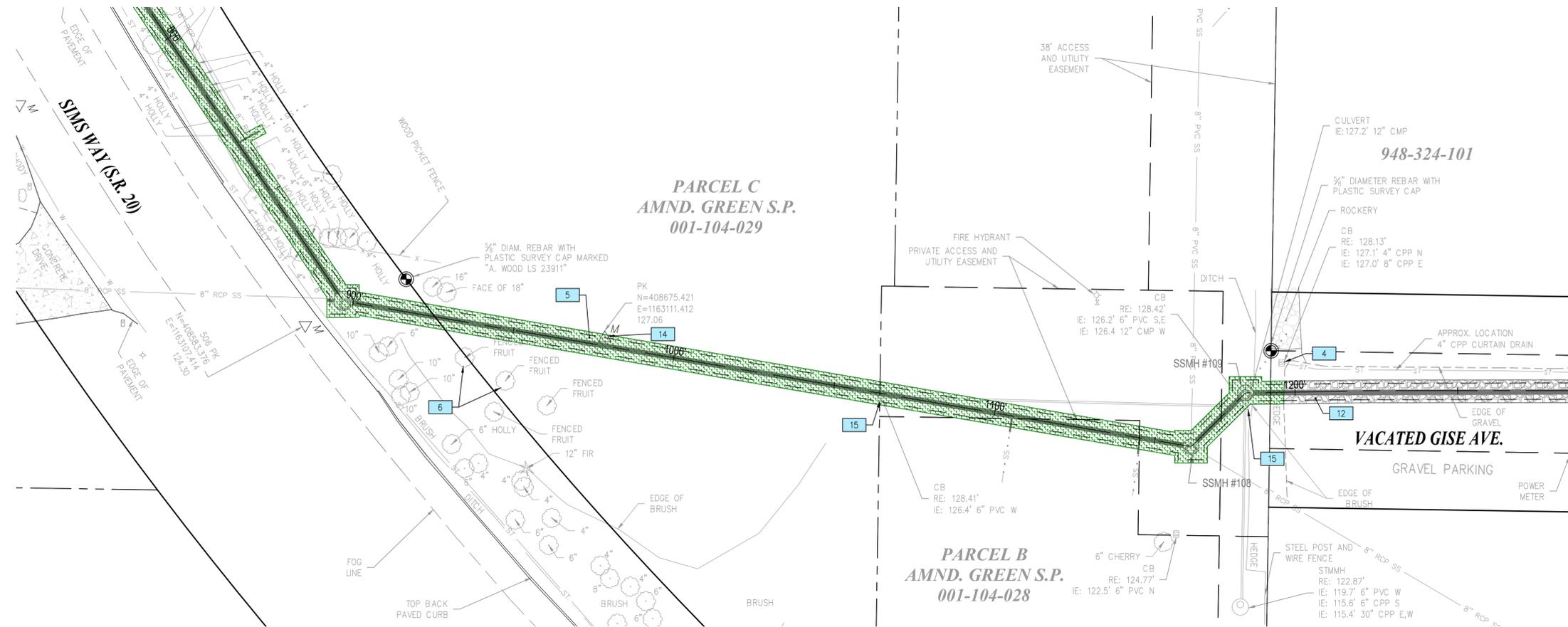
NO.	DATE	DESCRIPTION	BY	REVIEW

ENGINEER: CRB
DATE: Jan 22, 2026
CLIENT: TOWNSEND
JOB NO.: 25-0104

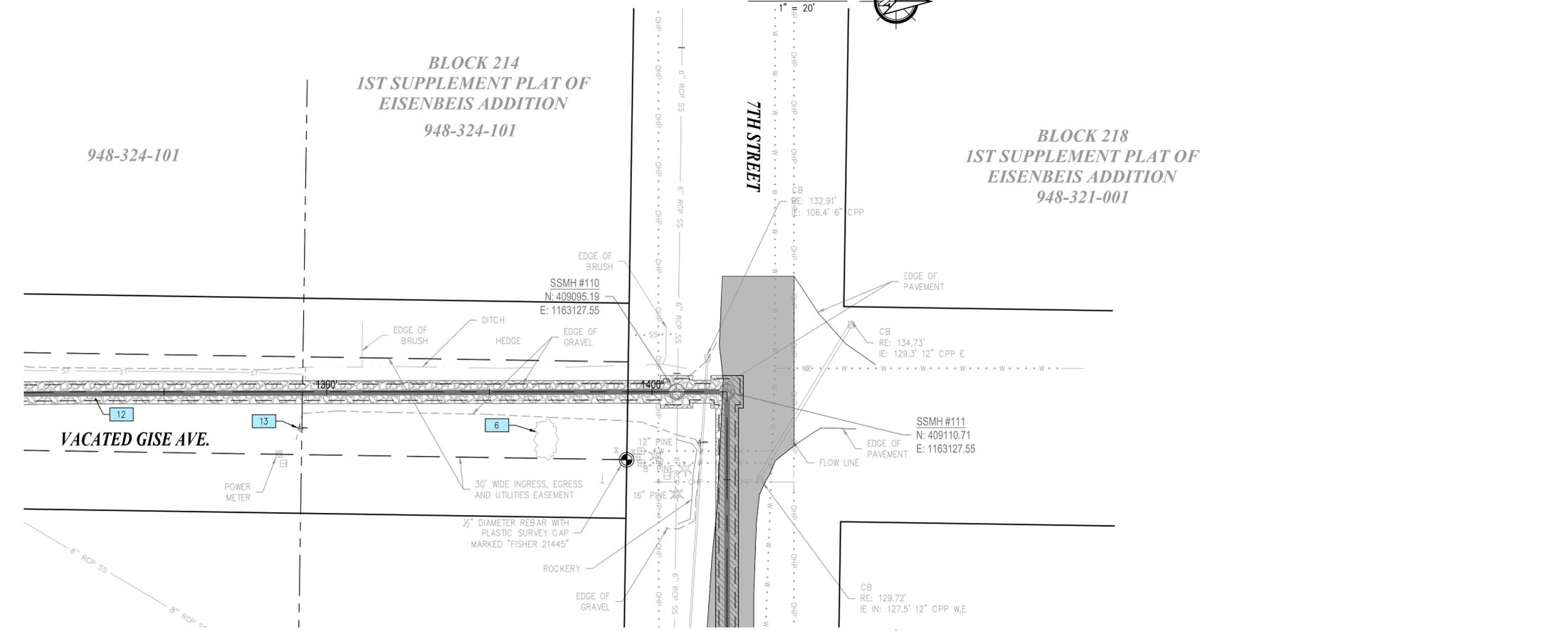
REVIEWED: RJF
DATE: Jan 23, 2026
FILENAME: HOL-D-RESTOP.DWG

SCALE: SHOWN

DWG NO.: R01
SHEET NO.: 15
19



PLAN VIEW



PLAN VIEW



RESTORATION NOTES

ALL RESTORATION LIMITS SHOWN ARE APPROXIMATE. ANY CONCRETE RESTORATION REQUIRED SHALL EXTEND TO THE NEAREST JOINT OR EDGE.

- APPROX. TRENCH LIMITS
- 3-INCH ASPHALT GRIND AND OVERLAY
- ASPHALT TRENCH PATCH OUTSIDE OF SIMS WAY
- ASPHALT TRENCH PATCH WITHIN SIMS WAY
- CEMENT CONCRETE SIDEWALK
- CEMENT CONCRETE CURB AND GUTTER
- CEMENT CONCRETE DRIVEWAY
- RESTORATION WITHIN NON-PAVED AREAS
- ASPHALT REPAIR IN DRIVEWAY/PARKING LOT
- GRAVEL

- 1 FULL ROAD OVERLAY. PROPOSED WORK, INCLUDING GRIND AND OVERLAY SHALL BE AS SHOWN ON THE PLANS, OR AS REQUIRED BY THE CITY.
- 2 TRENCH PATCH SURFACE RESTORATION PER SURFACE RESTORATION DETAIL FOR LOCAL ROADS ON DWG NO. D03, TYP.
- 3 TRENCH PATCH SURFACE RESTORATION PER SURFACE RESTORATION DETAIL FOR W SIMS WAY ON DWG NO. D03, TYP.
- 4 INSERT CATCH BASIN SILT SACK PER DETAIL ON DWG NO. D01. PROTECT CATCH BASIN AS NECESSARY TO CONSTRUCT PROPOSED IMPROVEMENTS. IF CATCH BASIN IS DAMAGED DURING CONSTRUCTION, REMOVE AND REPLACE CATCH BASIN WITH AN APPROVED EQUAL. PROVIDE NEW CATCH BASIN AND RISERS AS NECESSARY TO MATCH RIM AND INVERT ELEVATIONS
- 5 RESTORE DISTURBED SURFACE TO EXISTING OR BETTER CONDITION PER SURFACE RESTORATION IN NON-PAVED AREA DETAIL ON DWG NO. D03, TYP.
- 6 PROTECT TREES AS NECESSARY TO AVOID DAMAGE WHILE CONSTRUCTING PROPOSED IMPROVEMENTS PER TREE PROTECTION NOTES ON DWG NO. D01. IF CITY OR ENGINEER DETERMINES THAT TREE OR TREE ROOTS WERE SUBSTANTIALLY DAMAGED DURING CONSTRUCTION, ENTIRE TREE INCLUDING ROOTS/ROOT BALL SHALL BE REMOVED AND REPLACED WITH TOPSOIL. NOT ALL TREES THAT MAY BE IMPACTED ARE SHOWN/LABELED ON THIS DRAWING.
- 7 REMOVE EXISTING CONCRETE BARRIER AND REPLACE WITH ONE PRECAST TYPE F CONCRETE BARRIER PER WSDOT STANDARD PLAN C-60. 10-04.
- 8 RESTORE EXISTING ASPHALT DRIVEWAY. ASPHALT SHALL HAVE A THICKNESS OF 4".
- 9 RESTORE EXISTING CURB AND GUTTER. MINIMUM RESTORATION SHALL BE TO THE NEAREST JOINT, TYP.
- 10 RESTORE ROADWAY PAINT AND STRIPING, INCLUDING ALL CENTERLINES, TURN LANES, AND FOG LINES, TO EXISTING OR BETTER CONDITIONS, TYP.
- 11 RESTORE ROADWAY MARKINGS TO EXISTING OR BETTER CONDITIONS, TYP.
- 12 RESTORE GRAVEL ROADWAY PER DETAIL FOR GRAVEL ROADS ON DWG NO. D03, TYP. ROAD GRADING SHALL MATCH EXISTING CONDITIONS.
- 13 IF DISTURBED OR DAMAGED, REPLACE SIGN.
- 14 PRESERVE MONUMENT DURING CONSTRUCTION. IF DISTURBED DURING CONSTRUCTION, MONUMENT SHALL BE REPLACED IN ACCORDANCE WITH RECOGNIZED SURVEYING PRACTICES BY A LAND SURVEYOR LICENSED IN WASHINGTON.
- 15 REINSTATE/REPLACE STORM CATCH BASIN AND STORM PIPE FOLLOWING CONSTRUCTION OF SEWER.



CITY OF PORT TOWNSEND
HOLCOMB AND WILSON
SEWER REPLACEMENT

RESTORATION PLAN 2

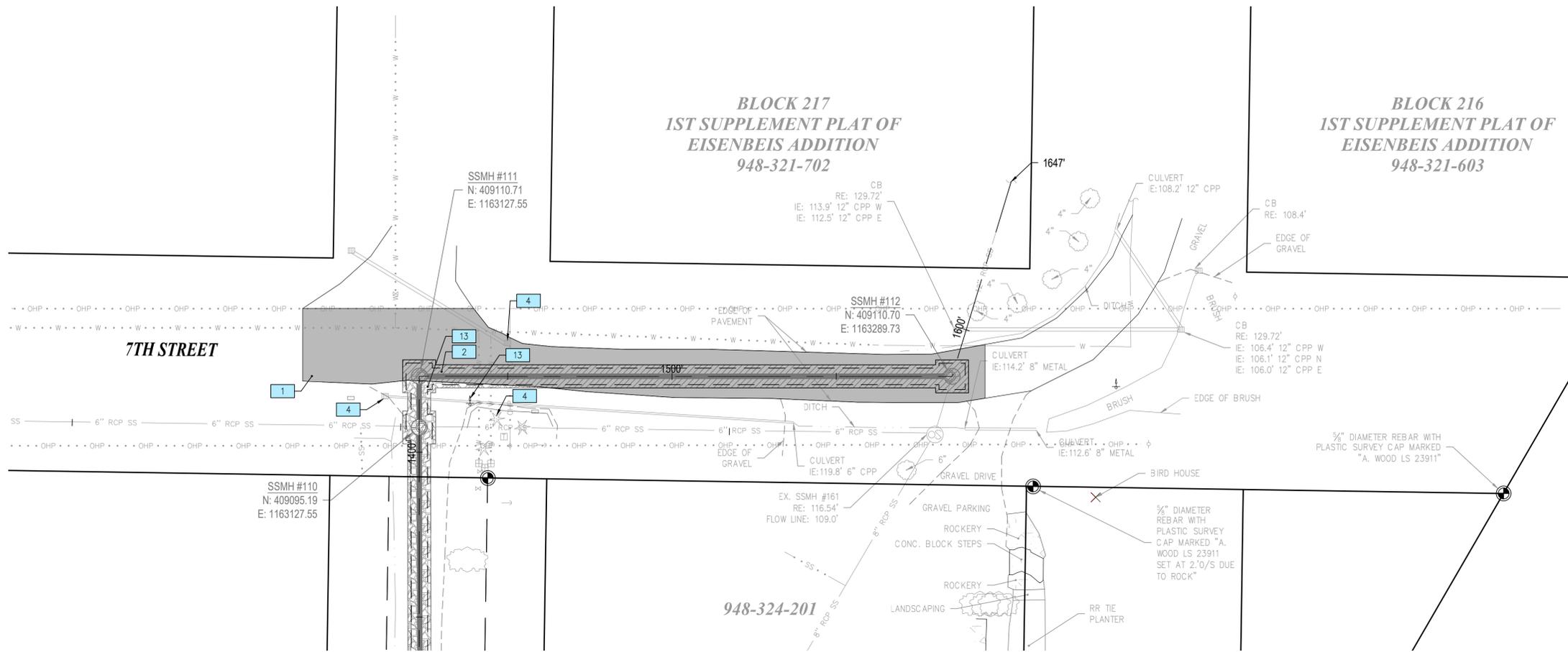


ENGINEER: CRB	DATE: Jan 22, 2026	CLIENT: TOWNSEND	JOB NO.: 25-0104	
REVIEWED: RJF	DATE: Jan 23, 2026	FILENAME: HOL-D-RESTOP.DWG		
REVISIONS				
NO.	DATE	DESCRIPTION	BY	REVIEW

SCALE: SHOWN	
DRAWING IS FULL SCALE WHEN BAR MEASURES 2"	
DWG NO.: R02	SHEET NO.: 16

**BLOCK 217
1ST SUPPLEMENT PLAT OF
EISENBEIS ADDITION
948-321-702**

**BLOCK 216
1ST SUPPLEMENT PLAT OF
EISENBEIS ADDITION
948-321-603**



PLAN VIEW
1" = 20'



RESTORATION NOTES

ALL RESTORATION LIMITS SHOWN ARE APPROXIMATE. ANY CONCRETE RESTORATION REQUIRED SHALL EXTEND TO THE NEAREST JOINT OR EDGE.

- APPROX. TRENCH LIMITS
- 3-INCH ASPHALT GRIND AND OVERLAY
- ASPHALT TRENCH PATCH OUTSIDE OF SIMS WAY
- ASPHALT TRENCH PATCH WITHIN SIMS WAY
- CEMENT CONCRETE SIDEWALK
- CEMENT CONCRETE CURB AND GUTTER
- CEMENT CONCRETE DRIVEWAY
- RESTORATION WITHIN NON-PAVED AREAS
- ASPHALT REPAIR IN DRIVEWAY/PARKING LOT
- GRAVEL

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- 2 TRENCH PATCH SURFACE RESTORATION PER SURFACE RESTORATION DETAIL FOR LOCAL ROADS ON DWG NO. D03, TYP.
- 3 TRENCH PATCH SURFACE RESTORATION PER SURFACE RESTORATION DETAIL FOR W SIMS WAY ON DWG NO. D03, TYP.
- 4 INSERT CATCH BASIN SILT SACK PER DETAIL ON DWG NO. D01. PROTECT CATCH BASIN AS NECESSARY TO CONSTRUCT PROPOSED IMPROVEMENTS. IF CATCH BASIN IS DAMAGED DURING CONSTRUCTION, REMOVE AND REPLACE CATCH BASIN WITH AN APPROVED EQUAL. PROVIDE NEW CATCH BASIN AND RISERS AS NECESSARY TO MATCH RIM AND INVERT ELEVATIONS
- 5 RESTORE DISTURBED SURFACE TO EXISTING OR BETTER CONDITION PER SURFACE RESTORATION IN NON-PAVED AREA DETAIL ON DWG NO. D03, TYP.
- 6 PROTECT TREES AS NECESSARY TO AVOID DAMAGE WHILE CONSTRUCTING PROPOSED IMPROVEMENTS PER TREE PROTECTION NOTES ON DWG NO. D01. IF CITY OR ENGINEER DETERMINES THAT TREE OR TREE ROOTS WERE SUBSTANTIALLY DAMAGED DURING CONSTRUCTION, ENTIRE TREE INCLUDING ROOTS/ROOT BALL SHALL BE REMOVED AND REPLACED WITH TOPSOIL. NOT ALL TREES THAT MAY BE IMPACTED ARE SHOWN/LABELED ON THIS DRAWING.
- 7 REMOVE EXISTING CONCRETE BARRIER AND REPLACE WITH ONE PRECAST TYPE F CONCRETE BARRIER PER WSDOT STANDARD PLAN C-60. 10-04.
- 8 RESTORE EXISTING ASPHALT DRIVEWAY. ASPHALT SHALL HAVE A THICKNESS OF 4".
- 9 RESTORE EXISTING CURB AND GUTTER. MINIMUM RESTORATION SHALL BE TO THE NEAREST JOINT, TYP.
- 10 RESTORE ROADWAY PAINT AND STRIPING, INCLUDING ALL CENTERLINES, TURN LANES, AND FOG LINES, TO EXISTING OR BETTER CONDITIONS, TYP.
- 11 RESTORE ROADWAY MARKINGS TO EXISTING OR BETTER CONDITIONS, TYP.
- 12 RESTORE GRAVEL ROADWAY PER DETAIL FOR GRAVEL ROADS ON DWG NO. D03, TYP. ROAD GRADING SHALL MATCH EXISTING CONDITIONS.
- 13 IF DISTURBED OR DAMAGED, REPLACE SIGN.
- 14 PRESERVE MONUMENT DURING CONSTRUCTION. IF DISTURBED DURING CONSTRUCTION, MONUMENT SHALL BE REPLACED IN ACCORDANCE WITH RECOGNIZED SURVEYING PRACTICES BY A LAND SURVEYOR LICENSED IN WASHINGTON.
- 15 REINSTATE/REPLACE STORM CATCH BASIN AND STORM PIPE FOLLOWING CONSTRUCTION OF SEWER.

**CITY OF PORT TOWNSEND
HOLCOMB AND WILSON
SEWER REPLACEMENT**

RESTORATION PLAN 3



NO.	DATE	DESCRIPTION	BY	REVIEW

ENGINEER: CRB
REVIEWED: RJF

DATE: Jan 22, 2026
JOB NO.: 25-0104

CLIENT: TOWNSEND
FILENAME: HOL-D-RESTOP.DWG

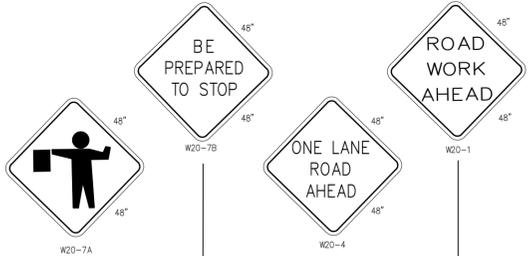
SCALE: SHOWN

DWG NO.: R03
SHEET NO.: 17

BUFFER DATA										
LONGITUDINAL BUFFER SPACE = B										
SPEED (MPH)	25	30	35	40	45	50	55	60	65	70
LENGTH (FEET)	155	200	250	305	360	425	495	570	645	-
BUFFER VEHICLE ROLL AHEAD DISTANCE = R										
TRANSPORTABLE ATTENUATOR:										100 FEET
MINIMUM HOST VEHICLE WEIGHT 15,000 LBS. THE MAXIMUM WEIGHT SHALL BE IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDATION.										
PROTECTIVE VEHICLE:										NO SPECIFIED DISTANCE REQUIRED
MAY BE A WORK VEHICLE STRATEGICALLY LOCATED TO SHIELD THE WORK AREA.										

SIGN SPACING = X(1)		
RURAL HIGHWAYS	60 / 65 MPH	800'
RURAL ROADS	45 / 55 MPH	500'
RURAL ROADS & URBAN ARTERIALS	35 / 40 MPH	350'
RURAL ROADS, URBAN ARTERIALS, RESIDENTIAL & BUSINESS DISTRICTS	25 / 30 MPH	200' (2)
URBAN STREETS	25 MPH OR LESS	100' (2)

1. ALL SPACING MAY BE ADJUSTED TO ACCOMMODATE INTERSECTIONS AND DRIVEWAYS.
2. THIS SPACING MAY BE REDUCED IN URBAN AREAS TO FIT ROADWAY CONDITIONS.



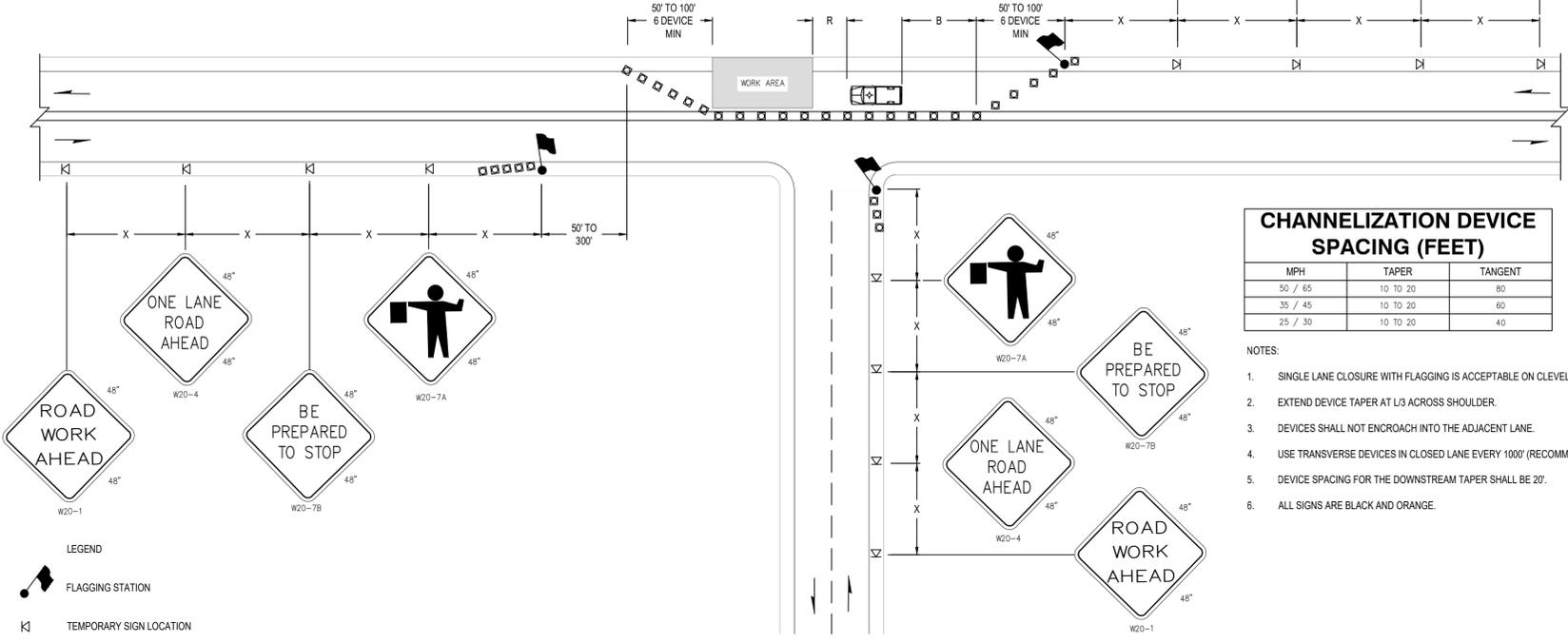
TRAFFIC CONTROL NOTES

1. SINGLE LANE CLOSURES ARE ACCEPTABLE FOR WORK ON CLEVELAND STREET AND 7TH STREET.
2. SHOULDER CLOSURES ARE ACCEPTABLE FOR WORK WITHIN THE W SIMS WAY RIGHT OF WAY. SEE DWG NO. TC02 FOR ALLOWABLE CLOSURES (WITH DETOUR) OF W SIMS WAY. SINGLE LANE CLOSURES OF W SIMS WAY REQUIRE ADVANCE APPROVAL FROM THE CITY.
3. MAXIMUM HOLDING TIME FOR SINGLE LANE CLOSURES IS 7 MINUTES.
4. CONTRACTOR SHALL FACILITATE SAFE ACCESS TO ALL DRIVEWAYS WITHIN THE WORK AREA.
5. SIGNS SHOWN ON THIS PLAN MAY NEED TO BE SUPPLEMENTED BY ADDITIONAL SIGNS DURING CONSTRUCTION BASED ON TRAFFIC USAGE AND NEEDS.
6. ALL SIGNS SHOWN ARE TO BE PLACED ON CLASS B POSTS. SOME SIGNS MAY REQUIRE COVERING DURING CERTAIN PHASES OF TRAFFIC CONTROL OPERATIONS.
7. CONTRACTOR SHALL REMOVE ALL SIGNING AND LEAVE THE SITE IN EXISTING CONDITION OR BETTER UPON PROJECT COMPLETION.

CHANNELIZATION DEVICE SPACING (FEET)		
MPH	TAPER	TANGENT
50 / 65	10 TO 20	80
35 / 45	10 TO 20	60
25 / 30	10 TO 20	40

- NOTES:
1. SINGLE LANE CLOSURE WITH FLAGGING IS ACCEPTABLE ON CLEVELAND STREET AND 7TH STREET.
 2. EXTEND DEVICE TAPER AT L/3 ACROSS SHOULDER.
 3. DEVICES SHALL NOT ENCR OACH INTO THE ADJACENT LANE.
 4. USE TRANSVERSE DEVICES IN CLOSED LANE EVERY 1000' (RECOMMENDED).
 5. DEVICE SPACING FOR THE DOWNSTREAM TAPER SHALL BE 20'.
 6. ALL SIGNS ARE BLACK AND ORANGE.

ONE LANE, TWO WAY TRAFFIC CONTROL WITH FLAGGERS
NOT TO SCALE



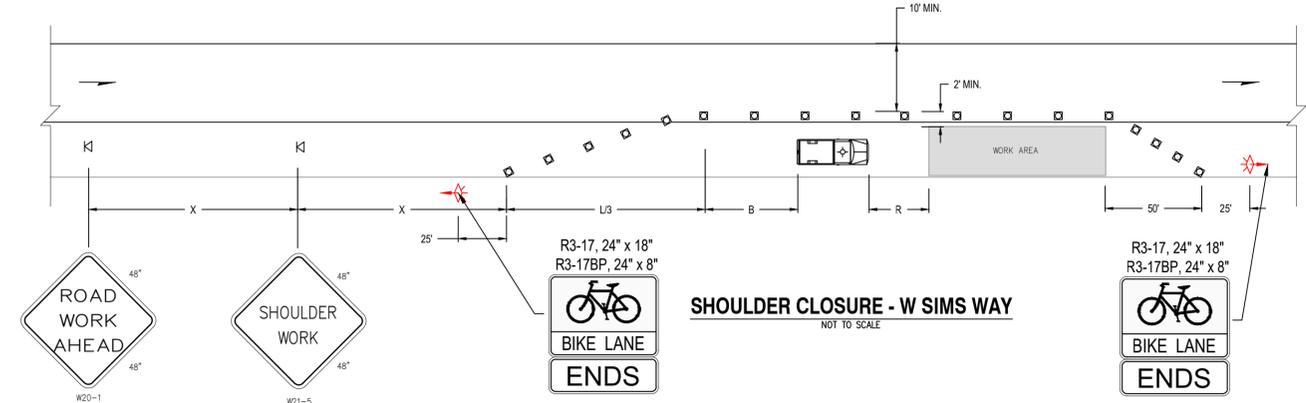
SIGN SPACING = X(1)		
RURAL ROADS & URBAN ARTERIALS	35 / 40 MPH	350'
RURAL ROADS, URBAN ARTERIALS, RESIDENTIAL & BUSINESS DISTRICTS	25 / 30 MPH	200' (2)
URBAN STREETS	25 MPH OR LESS	100' (2)

1. ALL SPACING MAY BE ADJUSTED TO ACCOMMODATE INTERSECTIONS AND DRIVEWAYS.
2. THIS SPACING MAY BE REDUCED IN URBAN AREAS TO FIT ROADWAY CONDITIONS.

SHOULDER WIDTH (FEET)	POSTED SPEED (MPH)									
	25	30	35	40	45	50	55	60	65	75
8'	40	40	60	90	-	-	-	-	-	-
10'	40	60	90	90	-	-	-	-	-	-

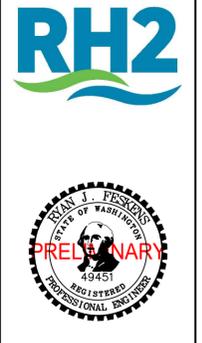
USE A MINIMUM 3 DEVICES TAPER FOR SHOULDER LESS THAN 8'.

CHANNELIZATION DEVICE SPACING (FEET)		
MPH	TAPER	TANGENT
35 / 40	30	60
25 / 30	20	40



BUFFER DATA										
LONGITUDINAL BUFFER SPACE = B										
SPEED (MPH)	25	30	35	40	45	50	55	60	65	70
LENGTH (FEET)	155	200	250	305	-	-	-	-	-	-
BUFFER VEHICLE ROLL AHEAD DISTANCE = R										
TRANSPORTABLE ATTENUATOR:										30 FEET MIN. TO 100 FEET MAX.
MINIMUM HOST VEHICLE WEIGHT 15,000 LBS. THE MAXIMUM WEIGHT SHALL BE IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDATION.										
PROTECTIVE VEHICLE:										NO SPECIFIED DISTANCE REQUIRED
MAY BE A WORK VEHICLE STRATEGICALLY LOCATED TO SHIELD THE WORK AREA.										

- NOTES:
1. DEVICE SPACING FOR THE DOWNSTREAM TAPER SHALL BE 20'.
 2. ALL SIGNS ARE BLACK ON ORANGE.



CITY OF PORT TOWNSEND
HOLCOMB AND WILSON
SEWER REPLACEMENT
TRAFFIC CONTROL PLAN I



ENGINEER: CRB	DATE: Jan 19, 2026	CLIENT: TOWNSEND	JOB NO.: 25-0104
REVIEWER: RJF	DATE: Jan 23, 2026	FILENAME: HOL-D-TC.DWG	
REVISIONS			
NO.	DATE	DESCRIPTION	BY



W20-3
48" x 48"



W20-2
48" x 48"



M4-9S
30" x 24"



M4-10L
48" x 18"



M4-10R
48" x 18"



M4-8A
24" x 18"



WSDOT TYPE III
BARRICADE

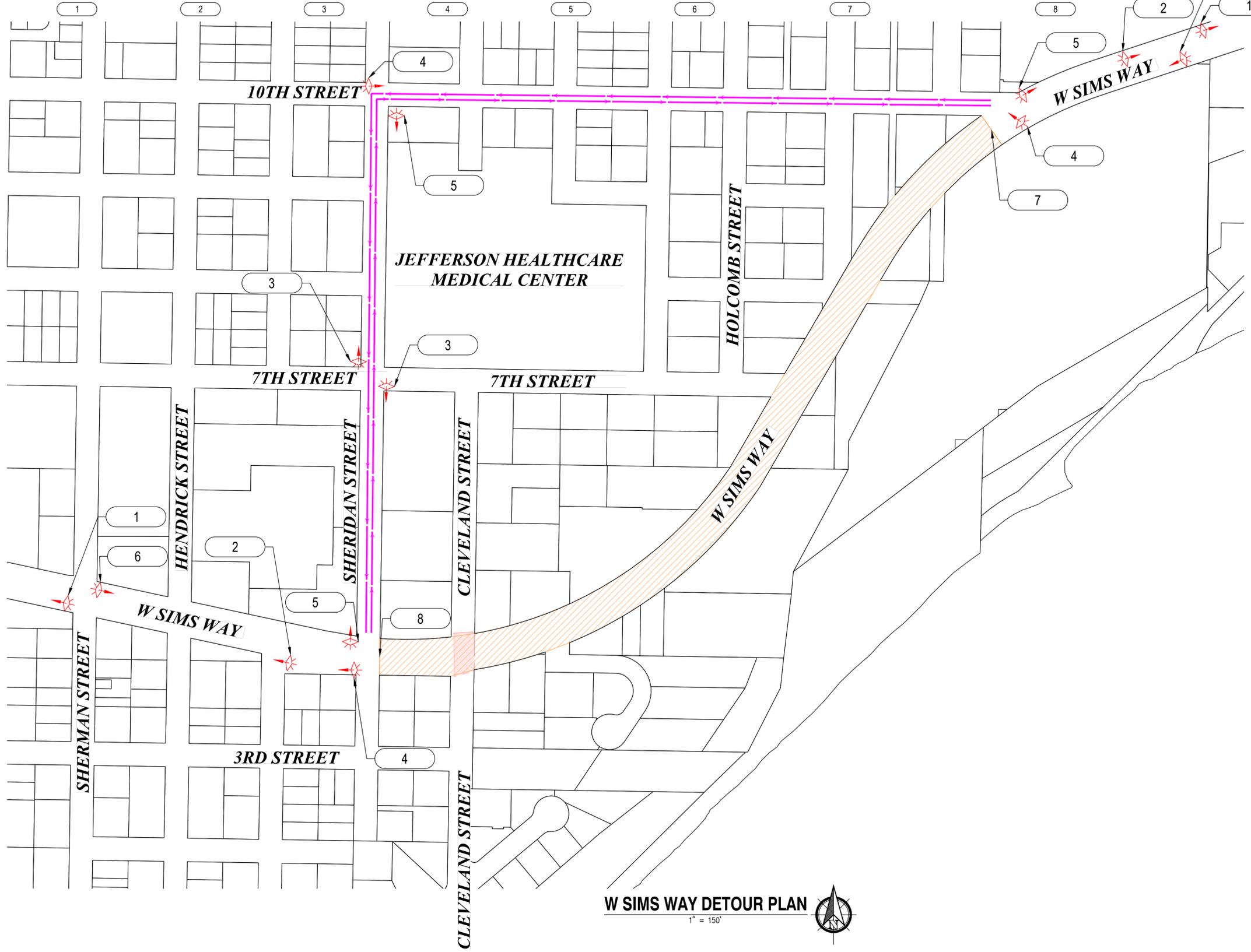


WSDOT TYPE III
BARRICADE

LEGEND	
	CLASS B CONSTRUCTION SIGN
	PROJECT WORK AREA
	DETOUR PATH
	ROAD CLOSURE AREA

W SIMS WAY CLOSURE NOTES

- CONTRACTOR TO SUBMIT TRAFFIC CONTROL PLAN TO CITY FOR APPROVAL AT LEAST **10 WORKING DAYS** PRIOR TO ANY ROAD CLOSURES.
- (2) SINGLE NIGHT CLOSURES OF W SIMS WAY WITH DETOUR ARE ACCEPTABLE TO INSTALL WATER AND SEWER PIPES UNDER THE ROAD. PROVIDE CITY AT LEAST 3 WORKING DAYS NOTICE PRIOR TO CLOSURES.
- NO OTHER FULL LANE CLOSURES OR SINGLE LANE CLOSURES ON W SIMS WAY WILL BE ALLOWED THROUGHOUT THE PROJECT. SEE DWG NO. TC01 FOR SHOULDER CLOSURES OF W SIMS WAY.
- ROAD CLOSURE HOURS: 7PM-5AM
- ALL TRAFFIC CONTROL DEVICES ARE REQUIRED TO MEET THE LATEST EDITION OF THE MUTCD.
- SIGNS SHOWN ON THIS PLAN MAY NEED TO BE SUPPLEMENTED BY ADDITIONAL SIGNS DURING CONSTRUCTION BASED ON TRAFFIC USAGE AND NEEDS. SIGN PLACEMENT SHOWN ON THIS PLAN IS APPROXIMATE. LOCATE SIGNS IN A MANNER TO AVOID CONFLICTS WITH DRIVEWAYS, SIDEWALKS, UTILITIES AND EXISTING SIGNAGE. ENSURE ALL TEMPORARY SIGNAGE IS CLEARLY VISIBLE TO DRIVERS. ALL SIGNS SHALL BE LOCATED WITHIN THE RIGHT-OF-WAY.
- ALL BARRICADES ARE TO HAVE AMBER FLASHING LIGHTS SECURELY ATTACHED TO THE BARRICADE PER WSDOT STANDARD PLAN K80.20-00.
- PROVIDE EMERGENCY VEHICLE ACCESS TO ALL ADJACENT PROPERTIES AND BUILDINGS WITHIN THE SCOPE OF THIS PROJECT. NOTIFY LOCAL POLICE AND FIRE DEPARTMENTS AT LEAST 48 HOURS IN ADVANCE OF ANY ACTIONS BY THE CONTRACTOR WHICH MAY AFFECT THEIR FUNCTIONS.
- DURING NIGHTTIME CLOSURES, CONTRACTOR SHALL PROVIDE ADEQUATE LIGHTING OF TEMPORARY TRAFFIC CONTROL SIGNS AND WORK AREA.
- CONTRACTOR SHALL FACILITATE SAFE LOCAL ACCESS THROUGHOUT THE CLOSURES.
- CONTRACTOR SHALL REMOVE ALL SIGNING AND LEAVE THE SITE IN EXISTING CONDITION OR BETTER UPON PROJECT COMPLETION.



W SIMS WAY DETOUR PLAN

1" = 150'



CITY OF PORT TOWNSEND
HOLCOMB AND WILSON
SEWER REPLACEMENT

TRAFFIC CONTROL PLAN II

ENGINEER	DATE	REVISIONS	NO.	DATE	DESCRIPTION	BY	REVIEW
CRB	Jan 19, 2026						
RJF	Jan 23, 2026						